

# IDAHO SPRINGS EAST END ACTION PLAN



JANUARY 20, 2017



## PROJECT TEAM



### City of Idaho Springs

City Hall  
1711 Miner Street, Idaho Springs, CO, 80452  
(303) 567-4421  
Contact: Alan Tiefenbach - Community Development Planner



### Planner - Project Lead

studioINSITE  
3457 Ringsby Ct, Suite 223, Denver, CO 80216  
(303) 433-7100  
Contact: Jim Leggitt - Principal

### Land Use Specialist

Clarion Associates  
621 17th Street, Suite 2250, Denver, CO 80293  
(303) 830-2890  
Contact: Don Elliott - Vice President



### Transportation Specialist

Charlier Associates  
2919 Valmont Road, Suite 206, Boulder, CO 80301  
(303) 543-7277  
Contact: Jim Charlier - President



### Economics Specialist

Economic & Planning Systems  
730 17th Street, Suite 630, Denver, CO 80202  
(303) 623-3557  
Contact: Dan Guimond - Principal







## ACKNOWLEDGMENTS

This plan would not have been possible without the guidance from and sincere interest that the Idaho Springs City Council and Planning Commission have offered. Additionally, the leadership from city staff was exceptional!

### City Council

Mike Hillman, Mayor  
 Troy Erickson, Ward 1  
 Tracy Stokes, Ward 1  
 John Curtis, Ward 2  
 Kate Collier, Ward 2  
 Jason Siegel, Ward 3  
 Bob Bowland, Ward 3

### Planning Commission

William Mehrer, Chair  
 Cindy Olson  
 Don Reimer  
 Kate Collier  
 Chuck Howard  
 Katherine Cook (Alternate)  
 John Young (Alternate)

### City Staff Leadership

Andrew Marsh, City Administrator  
 Alan Tiefenbach, Community Development Planner

Thank you to the numerous **business owners, property owners, and residents** that took time to attend stakeholder sessions and public meetings. Your feedback was essential to creating a plan that seeks to transform the East End into a place that this community can be proud of for generations to come!

Finally, a special thanks to the **Mighty Argo Team** for generously working side-by-side with us in order to coordinate big ideas and concepts for the East End. This catalytic project is so important for the city, and specifically the East End!





## CONTENTS

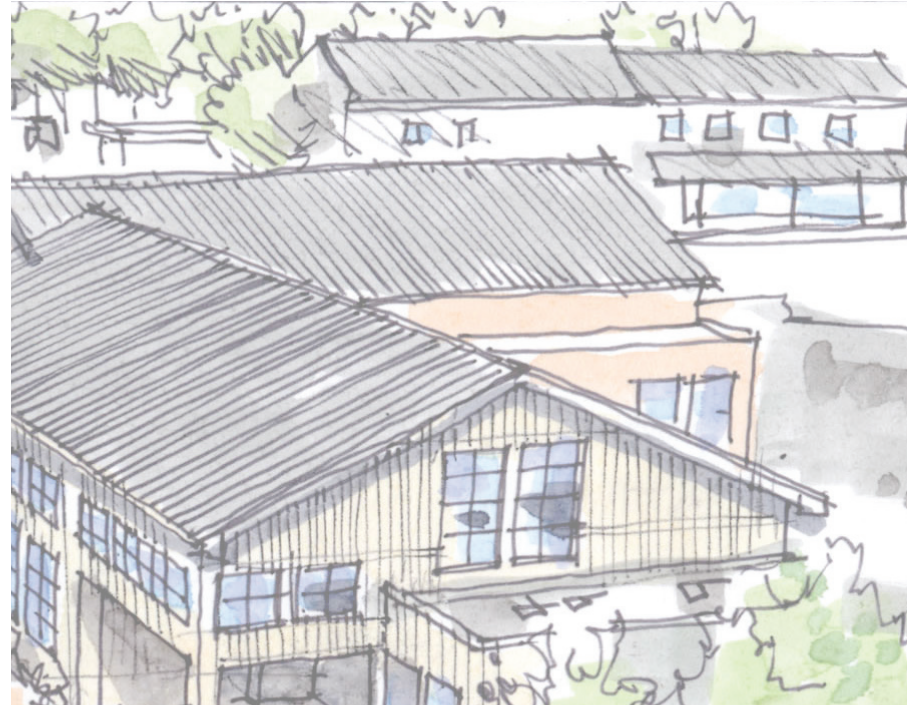
	Project Team	ii
	Acknowledgments	iii
	Contents	iv
<b>1</b>	<b>EXECUTIVE SUMMARY</b>	<b>1</b>
	Why Does the East End Need a Plan?	2
	What Does the Community Want?	3
	What is the Market Reality?	4
	Where Might Future Growth Go?	5
	What are the Barriers to Overcome?	6
	How Can We Achieve the Vision?	7
<b>2</b>	<b>PROJECT OVERVIEW</b>	<b>9</b>
	About This Plan	10
	How to Use This Plan	10
	Project Area	11
	Process	12
	Project Goals	13
	Design Principles and Guidelines	14
<b>3</b>	<b>URBAN DESIGN FRAMEWORK</b>	<b>21</b>
	Character Areas and Gateways	22
	Destinations and View Corridors	26
	Connectivity, Parking, and Transit	28
	Parks and Open Space	30
	Edges and Transitions	34
	Sun Access and Building Height	36





<b>4</b>	<b>STREETS AND PUBLIC SPACE</b>	<b>39</b>
	Street System Connectivity	40
	Trails System	42
	Street and Trail Sections	44
<b>5</b>	<b>DEVELOPMENT POTENTIAL &amp; ZONING</b>	<b>51</b>
	Overlay Zone	52
	Development Strategies	54
	Methodology	56
	Test Fit Sites	58
	Test Site A	60
	Test Site B	62
	Test Site C	64
	Test Site D	66
	Test Site E	68
	Vision Level Site 1	70
	Vision Level Site 2	72
	Vision Level Site 3	74
	The Mighty Argo	76
<b>6</b>	<b>IMPLEMENTATION STRATEGY</b>	<b>79</b>
	Introduction	80
	Priority Area 1: Community Enhancements and Placemaking	82
	Priority Area 2: Recreation and Tourism	84
	Priority Area 3: Parking Management	86
	Priority Area 4: Revitalization and Economic Development	88
	Priority Area 5: Housing	90
	Priority Area 6: Business Development	92







1

## Executive Summary



## WHY DOES THE EAST END NEED A PLAN?

The East End of Idaho Springs is undergoing a major public infrastructure investment with the rebuilding of Colorado Boulevard from an auto-dominated state highway that bypasses downtown, to a pedestrian-friendly city street that connects into downtown and adjacent neighborhoods. Private development will follow, and therefore, the city wishes to establish a vision and redevelopment strategy for the East End. In a nutshell, this plan answers the following questions:

- 1) What Does the Community Want?
- 2) What is the Market Reality?
- 3) Where Might Future Growth Go?
- 4) What are the Barriers to Overcome?
- 5) How can We Implement the Vision?







## WHAT DOES THE COMMUNITY WANT?

Through a series of community events, open houses, and stakeholder interviews, the following primary goals were established:

- 1) **Provide Access to the Creek** - there is a desire to feel a connection to Clear Creek, both visually (views) and physically (trail/street connections).
- 2) **Enhance Recreation Amenities** - create a culture of recreation, with regional hiking and biking trails, whitewater sports, and more!
- 3) **Create A Walkable Place** - enhance connectivity and appearance to better support the ability and desire to walk to destinations within the East End.
- 4) **Create An Authentic Place** - reflect the unique mining heritage and roadside character aesthetics. Don't create something that could be replicated anywhere!
- 5) **Offer More Housing Choices** - people who work in Idaho Springs can live here and so that people can age in place.
- 6) **Support Strong Businesses** - support businesses that contribute to the community and create a stronger tax base for the city and county.







## WHAT IS THE MARKET REALITY?

This plan looks at what the market reality is for new growth and how to achieve this reality within the community goals. While growth has been fairly stagnant in Idaho Springs in the last 10 years, there does seem to be a market for new growth in the next 5 to 10 years (see Appendix B), particularly in the following categories:

**1) Housing** - a goal of increasing the housing stock by 10 or 15 percent over the next 10 years is realistic. This would add between 100 and 150 housing units to the city, 50 to 75 of those units in the first five years. Projected housing prototypes include: single family detached for-sale workforce housing, multifamily affordable apartments, and market rate townhomes.

**2) Retail and Restaurants** - the estimated demand for retail in the East End is approximately 22,000 square feet of space. Due to the location and visibility from I-70, the majority of this space is seen as going toward fast casual restaurants and/or a brew pub. A smaller portion could go toward local-serving retail in mixed use buildings.

**3) Lodging** - There is potential for a 60 to 80 room, limited service hotel. This is the first level a national "flag" will build, and given the highway orientation and lack of "flag" hotels in Clear Creek County, this seems feasible.





## WHERE MIGHT FUTURE GROWTH GO?

Given the current building stock in the area, there is a lot of potential for redevelopment due to outdated structures and vacant or underutilized land. In order to identify locations for future development, the following data was used:

- 1) **Large parcels** - which parcels are large enough for a catalyst development project?
- 2) **Building to Value Ratio** - which parcels have a low building value on high opportunity sites (due to visibility, access, etc.)?
- 3) **Floor Area Ratio** - which parcels have a low building area compared to the site area?
- 4) **Vacant/Underutilized** - which parcels are currently vacant or underutilized, given their location and proximity to downtown and the interstate?

Using this methodology, a few areas within the East End rose to the top in terms of being feasible for redevelopment. In order to understand if the desired uses could fit within the sites, they were “tested” with conceptual programs and architectural massing. Multiple alternatives for each site were tested and “mixed and matched” to get to feasible corridor-wide mixture of uses.



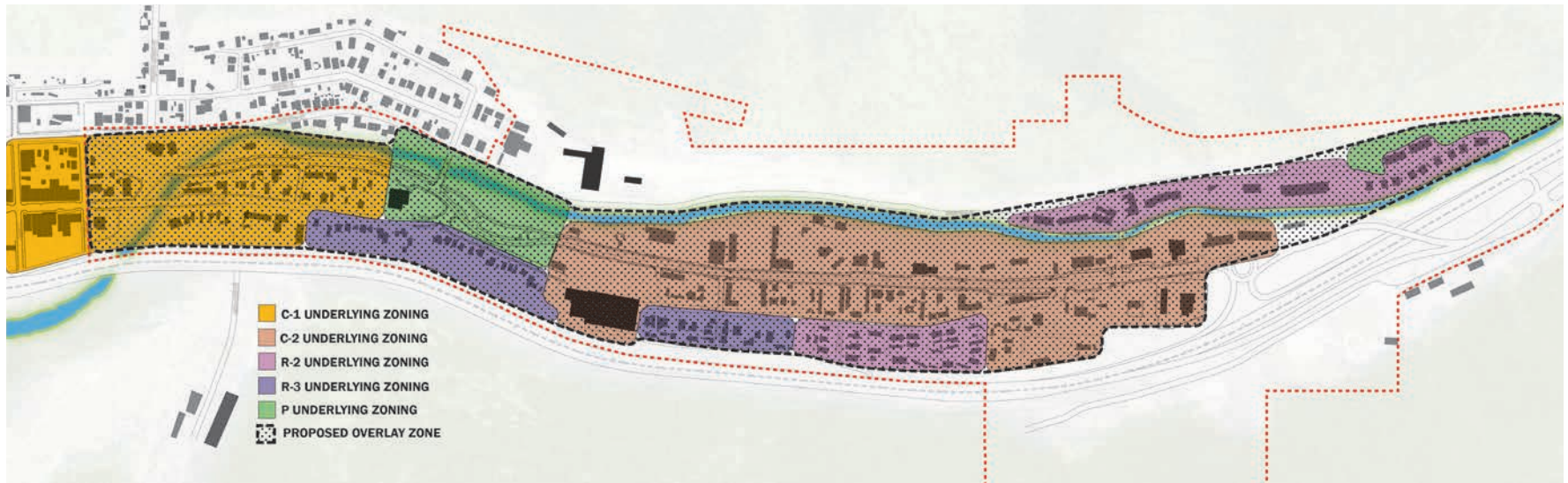


## WHAT ARE THE BARRIERS TO OVERCOME?

The future vision for the East End looks very different than today's East End. The new vision includes pedestrian-oriented streets and buildings with a mixture of uses to serve residents and visitors, as well as recreation opportunities. This is very different from the auto-oriented culture that exists today. While this plan shows multiple ways to achieve the future vision, there may be other ways as well. Staying true to the primary goals and establishing champions to lead the process will be key. A few challenges to consider and plan for include:

- 1) **Timing of Redevelopment is Not Certain** - *the city does not own major property in the East End, and therefore, timing of redevelopment will depend on when property owners are ready to sell, and when there is a willing buyer.*
- 2) **Implementation will Take Time** - *The entire vision will not happen overnight. The city will need to practice patience, along with determination. The Implementation Strategy (Chapter 6) outlines specific marching orders for the city to follow with the hopes of speeding up development.*
- 3) **Keeping Local Businesses is Important** - *many people make their livelihood with businesses in the East End. The city should remain vigilant and look for ways to assist existing businesses while attracting new ones as well.*
- 4) **Economies Change** - *currently, the Front Range is "booming" and there seems to be a market to support economic development in the East End. However, the economy could change in the next 5 or 10 years. The city should remain flexible to new market opportunities, as long as they meet the primary goals, as explained in this plan.*





An overlay zone ordinance was prepared as part of this plan to support redevelopment in the East End.

## HOW CAN WE ACHIEVE THE VISION?

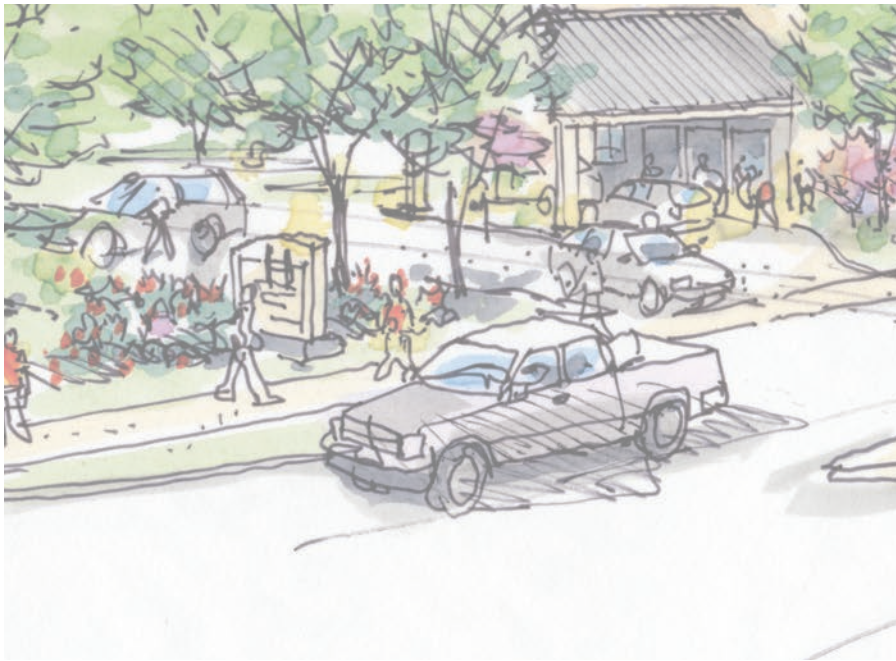
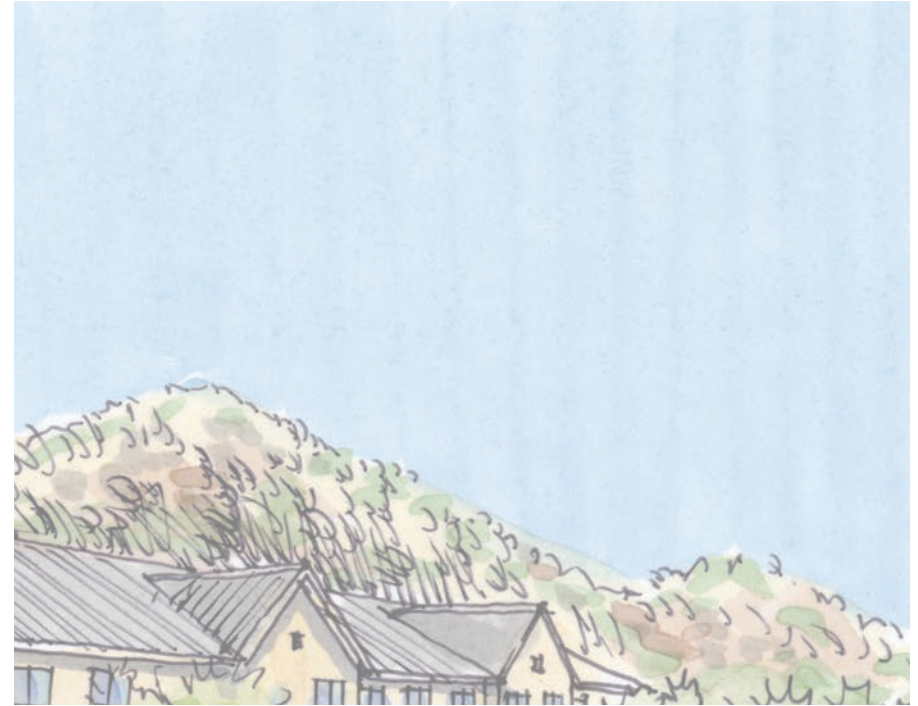
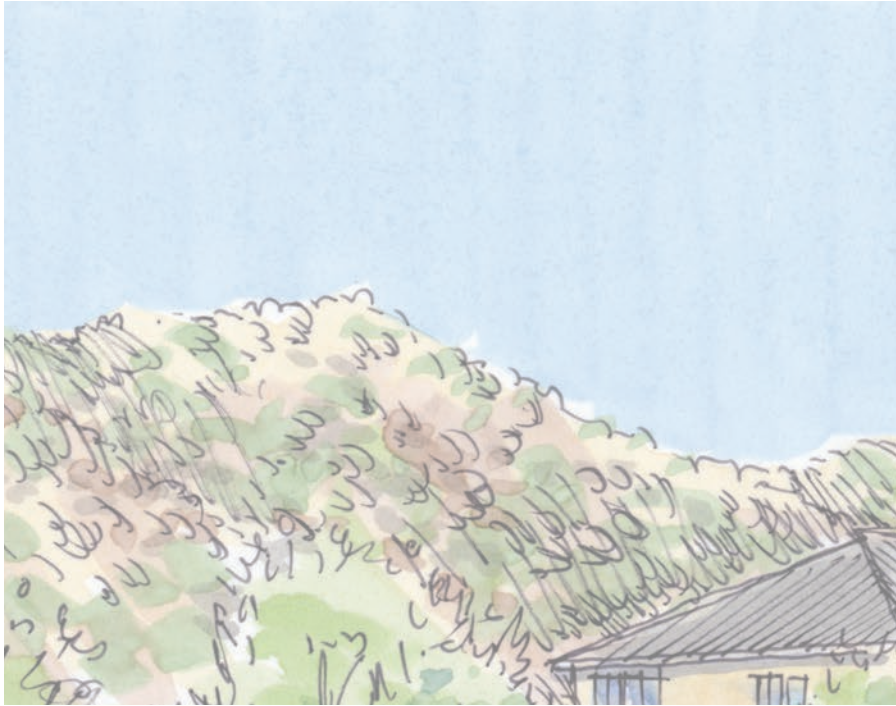
In order to implement the East End Action Plan, the city will need to establish policy that supports the goals herein and look for funding opportunities to help realize the vision. Below are some important policy-level action items:

- 1) **Create Overlay Zone** - create a new overlay zone that will encourage new development that meets the vision of the plan without the detriment to existing properties constructed with the underlying zoning (Appendix A).
- 2) **Update Comprehensive Plan** - establish citywide policies for improved infrastructure, transportation, land use, recreation, and housing that supports the goals of the East End Action Plan.
- 3) **Offer Incentives** - actively pursue creation of an Urban Renewal Authority to assist with property assemblage, project funding, and offer additional height for projects that provide affordable housing. The city is also actively pursuing funding in order to offer matching grants to assist small businesses for facade or property improvements that meet the goals of this plan.
- 4) **Educate the Public** - the city should work with existing business and property owners to educate them on the benefits of implementing this plan.
- 5) **Market Development Potential** - actively market the development potential by talking to developers and using forums such as Downtown Colorado, Inc., Department of Local Affairs, and other state and national resources and press to create interest in the East End.



The city is actively pursuing funding in order to offer matching grants to assist small businesses for facade or property improvements that meet the goals of this plan.







# 2

## Project Overview



EAST END ACTION PLAN

## ABOUT THIS PLAN

This plan is the result of a six-month, intensive planning process funded by the Colorado Department of Local Affairs (DOLA) and the City of Idaho Springs. It establishes a vision and redevelopment strategy for the “East End” of Idaho Springs - roughly from City Hall (17th Avenue) to Exit 241. This plan focuses on action items and strategies for implementing the vision in the short-term, within the next five years. The goal is to lay the foundation for public realm improvements and private redevelopment strategies for the East End so that implementation of these items can begin alongside or directly following reconstruction of Colorado Boulevard.

## HOW TO USE THIS PLAN

The City of Idaho Springs should use this plan to prioritize public infrastructure improvements as well as strategies to encourage redevelopment and/or beautification of private property within the study area.

This plan is also intended to be included as part of the future Comprehensive Plan Update for the City, which establishes policy goals for city-wide growth in terms of infrastructure, land use, recreation, transportation, and housing.

Property owners and developers should use this plan to ensure that individual projects meet the district-wide vision for the East End. Property upgrades, expansions, or redevelopments should consult this plan prior to making improvements. If unsure about the potential for a particular site, this plan establishes realistic opportunities that are rooted in market reality and economic feasibility.



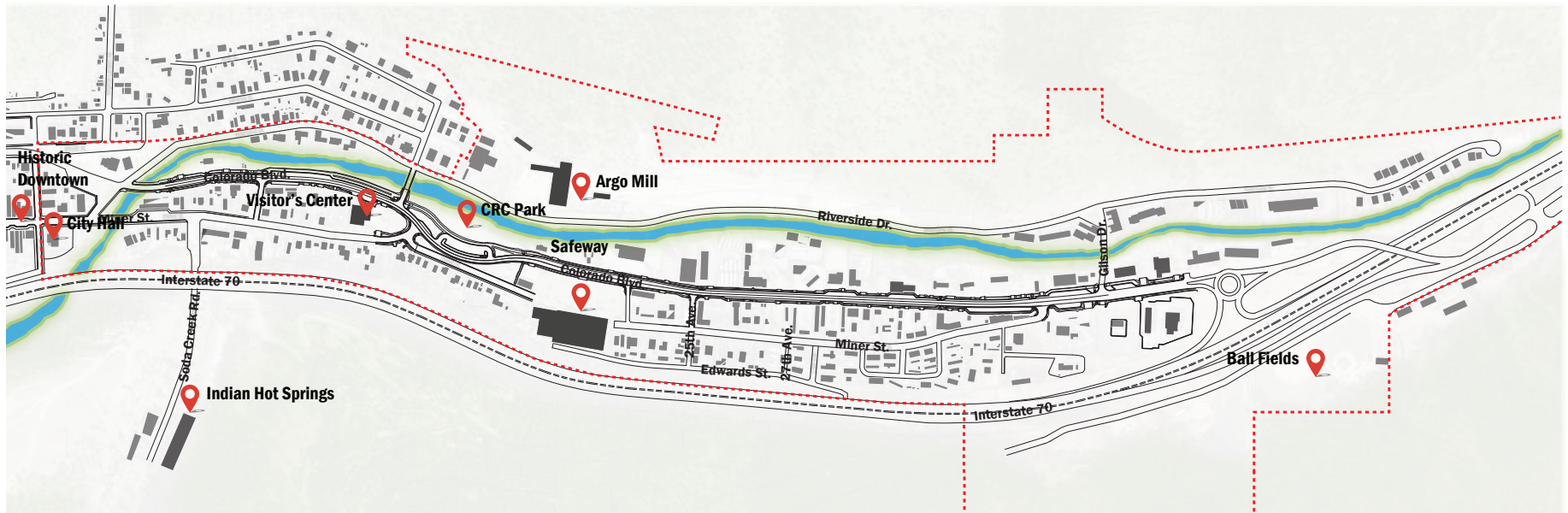


Figure 1 - Project Area

## PROJECT AREA

The project area (see Figure 1) generally includes everything east of City Hall within the city's core area. It is bound by 17th Avenue to the west, I-70 to the south (including the ballfields and adjacent land south of I-70), Exit 241 to the east, and Riverside Drive to the north (including the Argo Mill site north of Riverside Drive).

Popular sites in the project area include:

- 1) City Hall
- 2) The Visitor's Center
- 3) CRC Park
- 4) The Historic Argo Mill
- 5) Safeway
- 6) Ball Fields

Popular sites just outside the study area include:

- 1) Historic Downtown
- 2) Indian Hot Springs

### Colorado Boulevard:

*Colorado Boulevard, once a state highway owned and operated by CDOT, has been acquired by the city and is undergoing reconstruction. Phase 3 of the Colorado Blvd. reconstruction project includes the stretch within this project area, and is slated for completion by mid-2018. Throughout this document, Colorado Blvd. is represented showing the new alignment and design so that any proposed concepts align with the future Colorado Blvd. design.*

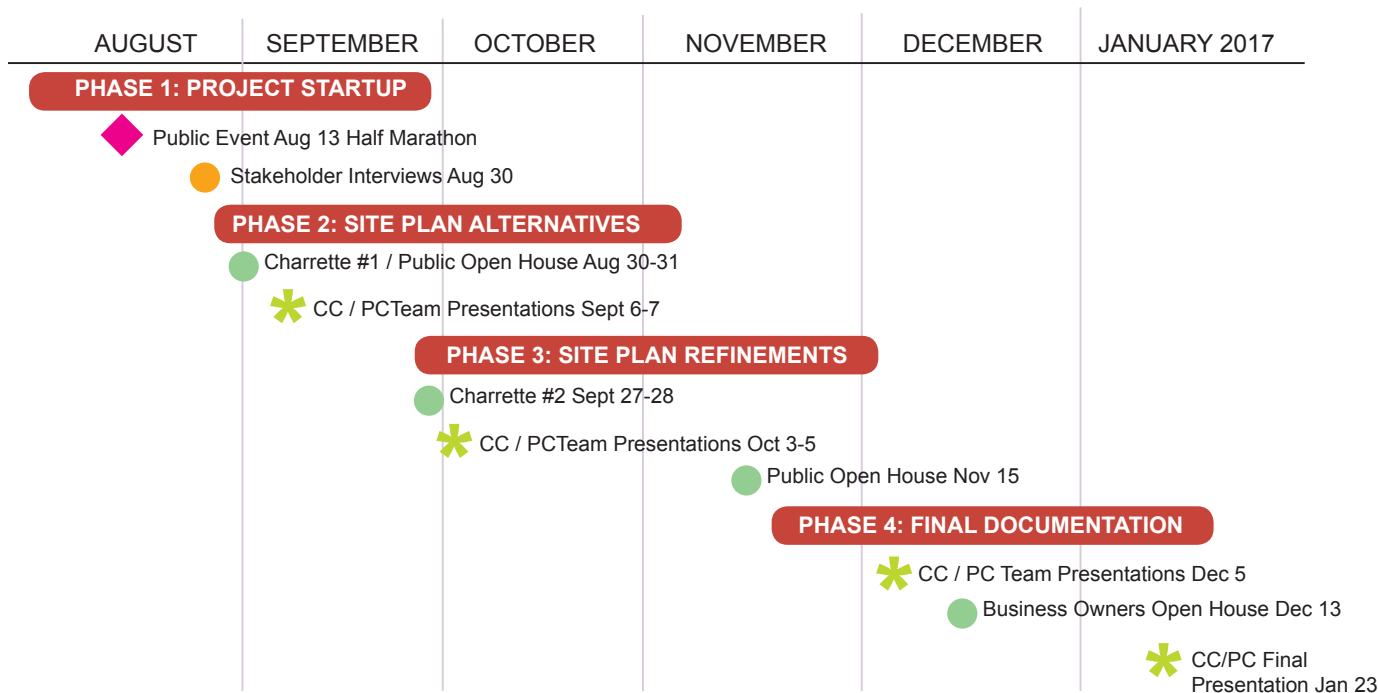


Figure 3 - Project Timeline



## PROCESS

This plan was created over the course of six months. It involved multiple check-ins with stakeholders, the citizens of Idaho Springs, and city leadership. It seeks to combine high-level goals with realistic solutions and implementation strategies. The general outline of the project process included:

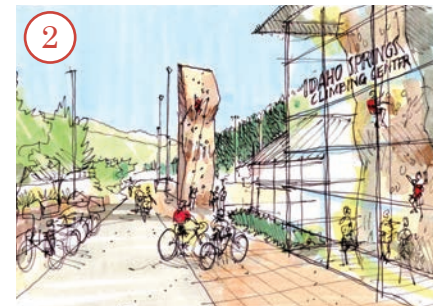
- 1) **Community Engagement** - the project began by asking the community what it desired for the future of the East End.
- 2) **Market Analysis** - next, a market analysis looked at trends and forecasted potential growth sectors.
- 3) **Project Goals** - goals were established to guide the project vision.
- 4) **Identify Opportunities** - The consultant team looked at where potential growth (new development) could occur, and then “test fit” some of those areas with development prototypes that the community wished to see and which were realistic from a market standpoint. The team also identified opportunities for public realm improvements such as new streets and connectivity, streetscapes, and public space.
- 5) **Implementation Strategy** - an implementation strategy establishes realistic measures for building out the vision and goals within this plan.

### Project Collaboration:

The design team for the East End Action Plan collaborated with multiple other projects happening within the East End Study Area to ensure plan concepts were complimentary to others. Collaboration with the design teams for the following projects were conducted:

- Colorado Blvd. Reconstruction
- Clear Creek Greenway
- “Mighty Argo” Redevelopment





## PROJECT GOALS

Based on the visioning and outreach process, the following project goals were developed:

- 1) **Provide Access to the Creek** - there is a desire to feel a connection to Clear Creek, both visually (views) and physically (trail/street connections).
- 2) **Enhance Recreation Amenities** - create a culture of recreation, with regional hiking and biking trails, whitewater sports, and more!
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- 5) **Offer More Housing Choices** - so that people who work in Idaho Springs can live here and so that people can age in place.
- 6) **Support Strong Businesses** - support businesses that contribute to the community and create a stronger tax base for the city and county.

Concepts and policy recommendations for achieving these goals are provided throughout this document. Each individual project, large and small, public or private, should seek to achieve these goals in order to support and build the greater vision for the East End.





## DESIGN PRINCIPLES AND GUIDELINES

In addition to the project goals, each individual project should also meet the following design principles and guidelines. These principles seek to further guide new development toward the preferred future vision. They should be used as a set of criteria to follow during the design process, in addition to the design standards required within the East End Overlay District. The design principles are listed below and described in further detail on the following pages through supported descriptions and guidelines:

- 1) **There is No Back Side**
- 2) **Design for Life at 3 MPH**
- 3) **Capture the Spirit of the East End**
- 4) **Contribute to a Better Quality of Life**
- 5) **Consider Future Generations**

### **East End Overlay District:**

*Redevelopment projects meeting a certain threshold must comply with the new East End Overlay District (Chapter 21, Article III, Section 46). The document is also included as Appendix A in this plan. The overlay district includes allowed uses, building and site regulations (setbacks and lot standards), parking requirements, and design standards. These design principles and guidelines should be considered, along with the requirements of the overlay district.*



## THERE IS NO BACK SIDE

There are four east-west axis' within the East End, along which new development should respond to: Clear Creek, Colorado Boulevard, Miner Street, and I-70. Therefore, careful consideration for design is desired on all sides of a building, i.e. there is no "back" side.

**1) Clear Creek** - properties along Clear Creek should engage and celebrate the creek for the experience and aesthetics of the creek. Safety and the adjacent floodplain should be key considerations when designing these spaces. Some examples for engaging the creek include:

- Natural areas with seating or picnic areas.
- Trail connections to the Greenway
- Gardens
- Well-lit areas
- Public or private spaces attached to a development overlooking creek, such as an outdoor deck or patio (ground floor) or balconies or roof decks (upper floors)

**2) Colorado Boulevard** - primary frontage for new development should orient to Colorado Boulevard. New development should be designed to help activate sidewalks along the street. Examples for activation include:

- Retail or restaurants (ground floor)
- Storefront windows (commercial)
- Stoops or front porches (residential)
- Sidewalk sales
- Cafe seating
- Benches
- Landscaping

**3) Miner Street** - many properties span between Colorado Blvd. and Miner Street. Miner Street east of the downtown core functions primarily as a quaint residential street, with single family homes and front porches fronting onto it. However, some of the larger parcels have turned their back to Miner Street which have resulted in it losing its sense of place and purpose. New development on the south side of Colorado Boulevard should also consider Miner Street a primary face by:

- Incorporating a residential use that fronts onto Miner Street
- If a residential use is not feasible, the architecture should "greet the street" with windows and doors
- Buildings along Miner Street should be set back from the street to reflect the residential nature.
- In some cases, Miner Street may be the best location for parking. When this is the case, the parking should be masked with landscaping, site walls, or other features.

**4) I-70** - the interstate is situated so much of the East End is visible from it, due to the topography. New development along I-70 should take this into consideration and improve appearance along I-70 by:

- Using varied roof forms that reflect the small-scale character of the neighborhood.
- Incorporating doors and windows on building faces that are visible.





## DESIGN FOR LIFE AT 3 MPH

When designing a new building or renovating an existing one, think about how the building will be perceived by a person walking by it instead of driving by it. This is especially important along Colorado Boulevard. The future vision of this commercial corridor is much different than its historic one. With the redesign of Colorado Boulevard, wide sidewalks will make it possible, even desirable, to walk along the corridor whereas before it was designed for automobiles and did not even have proper facilities for walking. Some guidelines for accommodating this principle include:

**Human-Scaled Design** - New or improved buildings should incorporate materials and methods that are scaled towards a human figure, that is designed to travel at 3 miles per hour rather than an automobile.

- Use materials that are designed to be installed by a person. Examples include masonry, panels (wood, metal, composite), and siding.
- Human-scaled materials provide for easy perception of a building's overall scale because the individual elements that make up the whole of the structure are understood by the observer.
- Use materials that are textural.
- Art, such as murals or sculpture, is another example of human-scaled design.
- The overall scale of a building should feel comfortable, and appear to fit in with the context of the East End.
- Mass a building so that it provides interest and includes composition. Use smaller masses toward sidewalks where people most closely interact with a building.
- Signage should be sized to reinforce the pedestrian-oriented nature of the street. For example, billboards, revolving, monument, and digital signs are not appropriate.

**Attention to Detail** - New or improved buildings should incorporate elements that have clear intention and are designed to be viewed up close. Examples include:

- High quality awnings, canopies, pergolas, arcades (or similar) for protection from the elements.
- A defined "base" to a building that serves as a visual foundation for where the building meets the ground.
- Inset windows and/or a pronounced sill or header.
- Varied use of high quality materials. However, this should not be overdone - too many material changes can appear cluttered.
- Site design should also use attention to detail through varying plantings, seasonal flowers (in boxes or pots), ground plane materials, and furnishings.
- Signage should be graphically appealing and designed to fit in with the architectural aesthetics.



Use smaller masses toward sidewalks where people most closely interact with a building.



The overall scale of a building should feel comfortable, and appear to fit in with the context of the East End.



Use materials that are designed to be installed by a person. Human-scaled materials provide for easy perception of a building's scale.



Mass a building so that it provides interest and includes composition.



New or improved buildings should incorporate elements that have a clear intention and are designed to be viewed up close. High quality awnings, canopies, pergolas, arcades (or similar) for protection from the elements is an example.



Signage should be sized to reinforce the pedestrian-oriented nature of the street. Signage should also be graphically appealing and fit in with the architecture.



## CAPTURE THE SPIRIT OF THE EAST END

The East End has an eclectic, yet inherent character - from the historic Argo Mill and Victorian miner housing to the more traditional mountain aesthetic to Googie Signage along Colorado Boulevard. These varied styles help tell the story of the East End. To the extent feasible, new development should take cues from the existing styles, yet still appear “of their own time” to contribute to the East End storytelling of the future.

**Traditional Aesthetic** - when surveyed, the community seemed to choose traditional style architecture over more contemporary styles. People had more positive notions toward two and three-story brick buildings (much like downtown) and one to two-story single family homes or cottages (much like the historic neighborhoods of Idaho Springs). Some examples of appropriate, but contemporary expressions of traditional architecture of Idaho Springs include:

- Using brick as a primary material on commercial and mixed use buildings. This is especially appropriate along Colorado Boulevard and closer to downtown.
- Using siding and pitched roofs on residential buildings.

**Mill Aesthetic** - the rich mining heritage can be seen in the historic Argo Mill within the East End. New buildings might capture the spirit of this history by incorporating a more industrial, mill architecture aesthetic. This contemporary interpretation is common throughout Colorado.

- Steel and metal siding are common materials that represent this aesthetic.
- Shed roofs and clerestory windows are also a common feature in the mill aesthetic.

**Googie Aesthetic** - Googie architecture, also known as “Doo Wop”, is a style that originated in the 1940’s and includes buildings and signage. Common features include curving or sharp geometric shapes, lots of glass and steel, and neon. This style is more pronounced in some of the signage along Colorado Boulevard than in buildings. Examples of contemporary expression of Googie style include:

- Prominent use of steel and glass.
- Restoration of existing signs or new signs that are indicative of that era, through use of neon (or LED made to look like neon)
- Sharp geometric shapes or curving roofs.



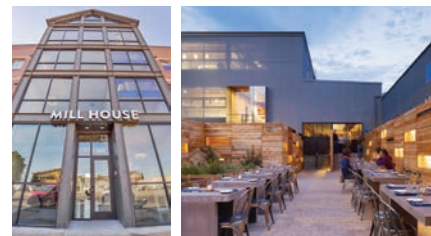
The varied styles of architecture in the East End help tell its story.



Using brick as a primary material on commercial and mixed use buildings is appropriate.



Using siding and pitched roofs on residential buildings is appropriate.



Steel and metal siding with shed roofs are common features of the mill aesthetic.



Curving roofs and lots of glass are exemplary of Googie architecture.



New signage that is indicative of the Googie era is appropriate.



## CONTRIBUTE TO A BETTER QUALITY OF LIFE

Quality of life is an important theme for Idaho Springs as a small mountain town. Elements that currently positively contribute to the quality of life include: small town atmosphere where everyone knows their neighbors, good schools, a charming historic downtown, proximity to nature, access to mountains, and more. New development in the East End should seek to contribute to a better quality of life for all of Idaho Springs residents. Some examples of this include:

**Design for All** - design new projects with everyone in mind, including residents, tourists, passersby, and people of all ages, abilities, and incomes.

- Design new projects for people of all ages, including children, parents with small children, teenagers, young couples, professionals, and seniors. For example, providing seating areas outside a building for seniors or whimsical art for children to enjoy.
- Consider people of all incomes - do not design toward only the upper, lower, or middle class with regards to housing types or destinations.
- Design projects to accommodate people with disabilities. This includes people in wheelchairs, the hearing and visually impaired, as well as those who may need assistance walking. Having ADA accessible ramps, smooth surfaces, and elevators (where applicable) is important. Using texture and sound are also appealing for the visually/hearing impaired.

**Encourage Walking** - walking is not only fun and efficient in a small community, it is also good for your health. New developments should encourage walking by providing multiple connections to/from and around the property. Providing shelter from the elements, prominently announcing main entrances, and activating the sidewalk edge also makes walking easier and more enjoyable.

- Incorporate uses that activate the sidewalk edge by engaging passersby wherever feasible, such as cafe seating, public art, seating areas, high transparency, or front porches.
- Provide multiple connections to and from a property. Highlight important connections such as to the creek, a front door, or to a parking area.

**Provide Choices** - part of what makes life so enjoyable is having choices - being able to choose where to eat dinner, buy groceries or gifts, how to get from place to place, or where to live are just a few examples. New developments should contribute to giving people choices.

- Avoid a monoculture of uses in the East End, such as all commercial or all residential. Instead, provide “choices” with a mixture of uses.
- Housing choices are important and contribute to a better quality of life for all. Provide a mixture of housing types and for a variety of incomes.



Design new projects for people of all ages, including children, parents with small children, professionals, and seniors. For example, providing seating outside a building for seniors or whimsical art for children to enjoy.



Incorporate uses that activate the sidewalk edge by engaging passersby wherever feasible, such as cafe seating, public art, seating areas, high transparency, or front porches.



Avoid a monoculture of uses in the East End, such as all commercial or all residential. Instead provide “choices” with a mixture of uses.



## CONSIDER FUTURE GENERATIONS

Development within the East End should be built to last - meeting the needs of today without sacrificing the needs of future generations. New projects should incorporate construction methods and features that reduce energy, conserve resources, and minimize environmental impacts. They should also be built to stand the test of time - with the ability to change uses over time. New projects should also reduce the risk of hazards, such as flooding and fires.

**High Quality, Durable Materials** - design buildings with significant life spans in order to reduce material waste. Use materials that can withstand the test of time and are appropriate for the harsh Colorado climate.

- Use materials that are proven to withstand the harsh Colorado mountain climate and can withstand significant thermal expansion and contraction.
- Examples of appropriate durable materials include: masonry, steel, concrete, and sealed metal and wood.
- Avoid non-durable materials such as non-cementitious stucco and materials that buckle or become degraded from thermal expansion and contraction.
- Use skilled labor to ensure correct installation of materials.

**Energy Conservation** - site and design buildings to take advantage of passive heating and cooling opportunities and to reduce overall energy demands.

- Site a building to maximize sun exposure for daylighting and solar gain. This is especially important in Idaho Springs, due to the “canyon effect” created by mountains on either side of the valley.
- Use materials that store and transfer heat through thermal mass, or use high quality insulation to avoid heat loss.
- Maximize natural ventilation with proper separation of buildings and operable windows.
- Consider incorporating renewable energy generation such as solar photovoltaics or wind turbines (small scale) for electricity or solar hot water.

**Reduce Waste and Water** - design a building to reduce waste, both during and post-construction. This includes:

- Use sustainable materials that are proven to reduce waste and water during their production and post-consumer life span.
- Reduce sending toxic chemicals into the storm drains and downstream by incorporating temporary storage and treatment of water on-site with bioswales or similar landscape treatment.
- Implement programs that reduce solid waste on-site. This includes recycling and composting.
- Consider use of permeable materials on a site to allow stormwater and snow to percolate through the surface and into the groundwater table, rather than being forced into the storm sewer.



Design buildings with significant life spans in order to reduce material waste. Use materials that can withstand the test of time and are appropriate for the harsh Colorado climate.



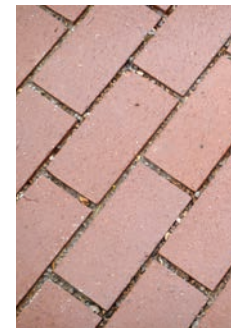
Site a building to maximize sun exposure for daylighting and solar gain.



Maximize natural ventilation with proper separation of buildings and operable windows.



Consider incorporating renewable energy generation such as solar photovoltaics or wind turbines for electricity or solar hot water.

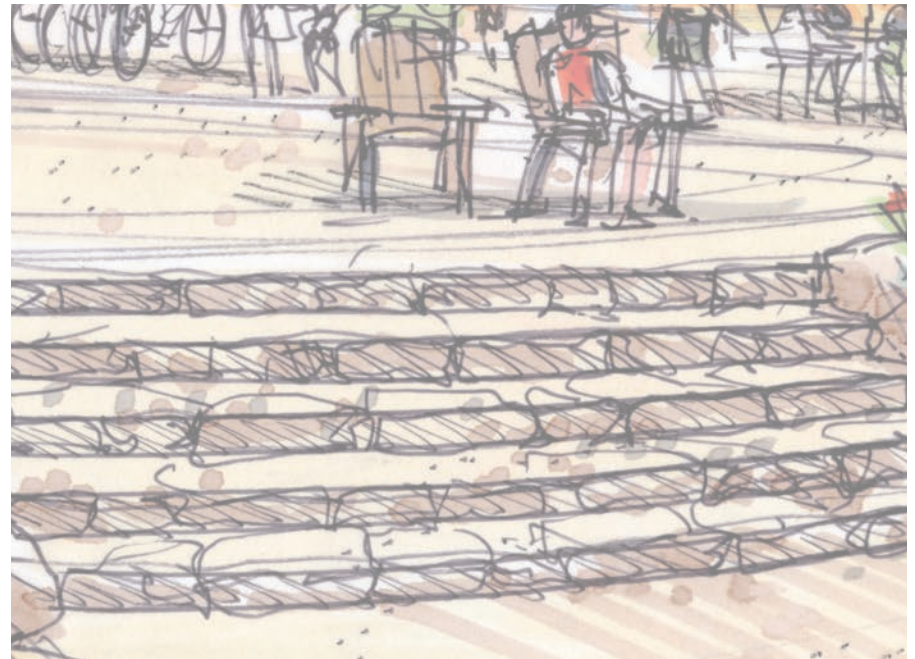


Consider the use of permeable materials on a site to allow stormwater and snow to percolate through the surface.



Reduce sending toxic chemicals into the storm drains and downstream by incorporating temporary storage and treatment of water on-site with bioswales and similar landscape treatment.







3

Urban Design Framework

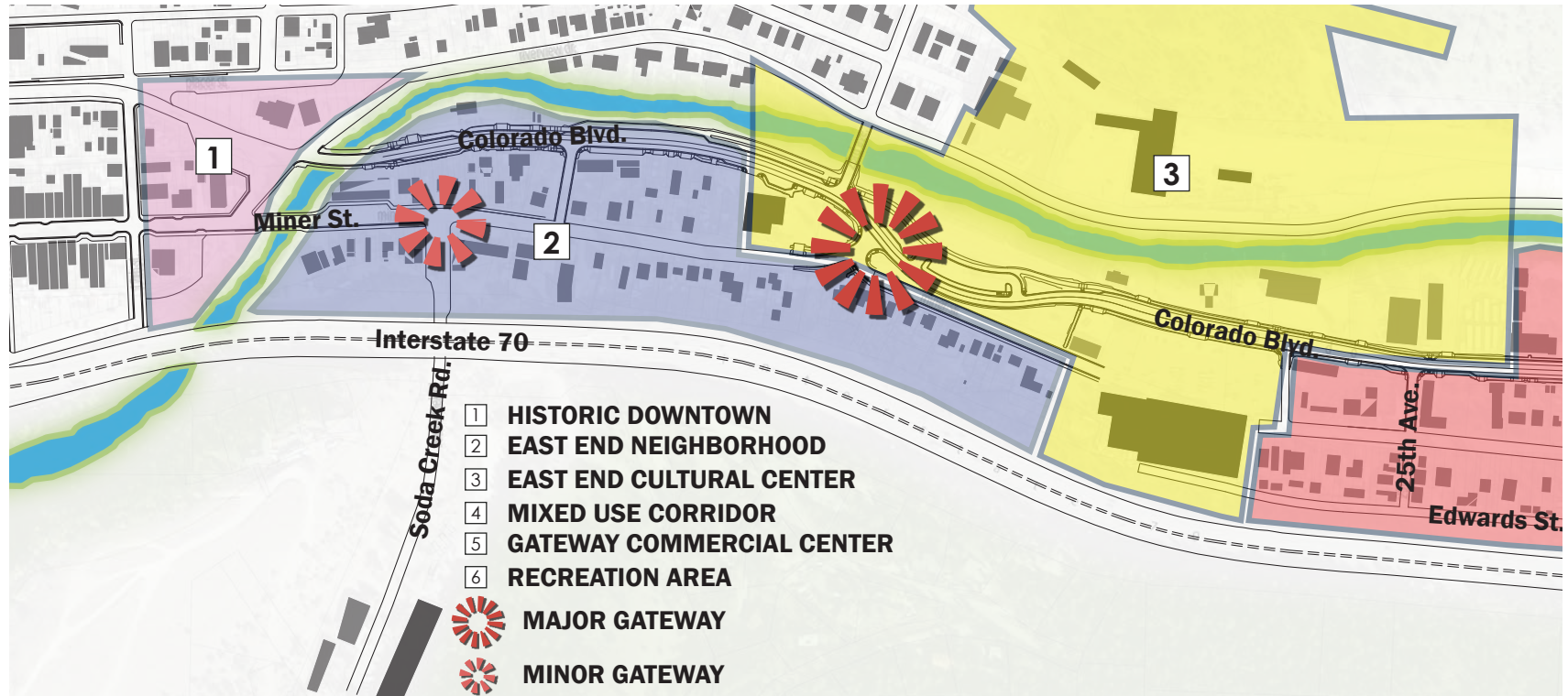


Figure 2 - Districts and Gateways

## CHARACTER AREAS AND GATEWAYS

Rather than defining this area as one long, continuous corridor, this plan identifies six “character areas” that make up the East End. They are defined based upon their existing (and future) characteristics, such as land use and architectural character. The following pages describe them in more detail. They include:

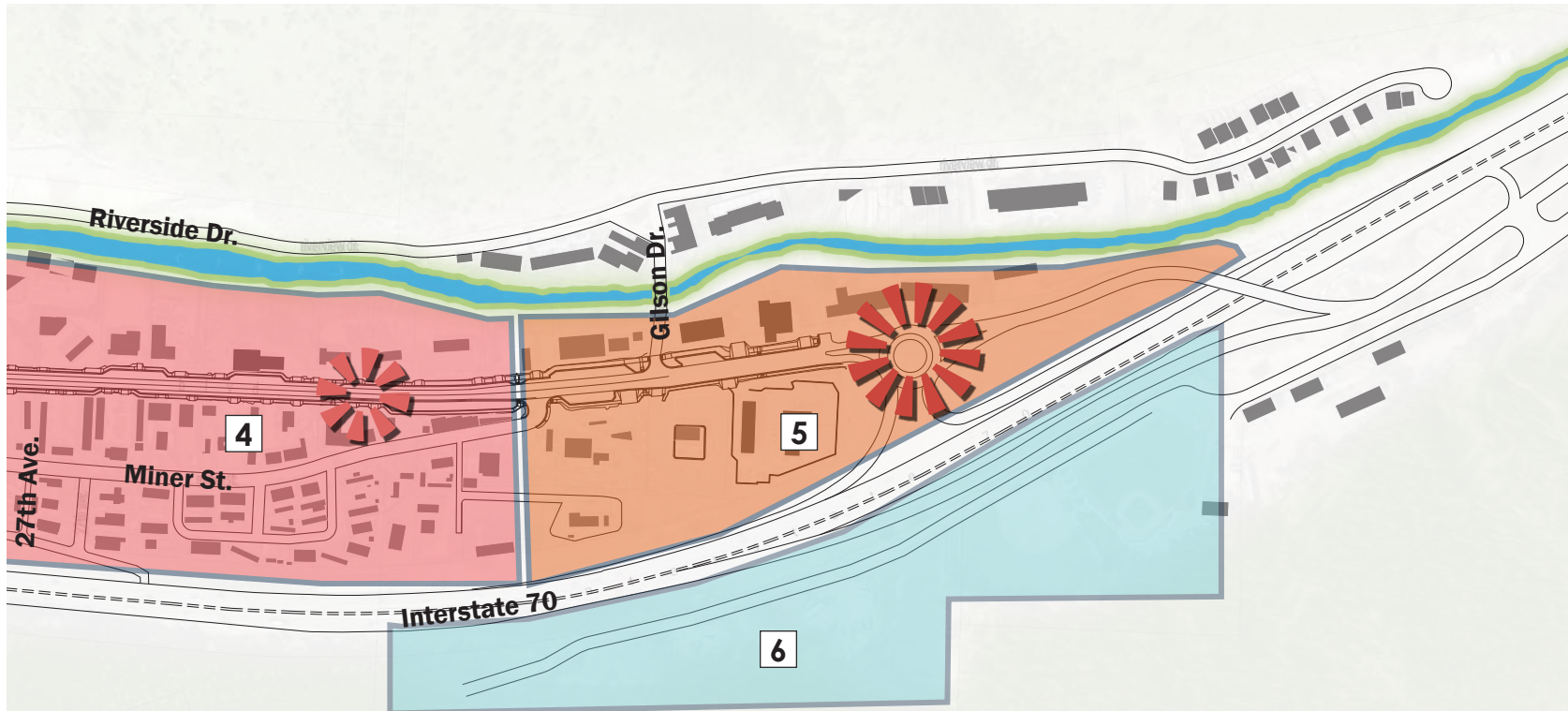
- 1) Historic Downtown
- 2) East End Neighborhood
- 3) East End Cultural Center
- 4) Mixed Use Corridor
- 5) Gateway Commercial Center
- 6) Recreation Center

Two major gateways and two minor gateways are also identified, which are intended to indicate areas where signage and beautification is desired to signify entrances into the East End. The major gateways are located at the Exit 241 Roundabout and near the Visitor’s Center where the new Colorado Boulevard will intersect Miner Street. Minor gateways are located at the intersection of Soda Creek Road and Miner and at Colorado Boulevard and the proposed intersection with “Argo” Road (see Figure 4).

### Character Area Intents:

*The boundaries shown for the character areas are not regulated by the existing underlying or new overlay zoning code. Rather, they were determined based on the existing (or proposed) desired character and are meant to guide new development to better respond to existing context.*

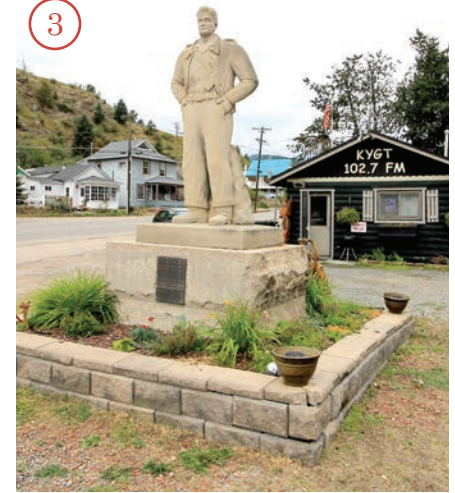




## 1 - HISTORIC DOWNTOWN

This area includes everything west of Clear Creek as it migrates under I-70, creating a defined border. This area is characteristic of Idaho Springs' historic architecture and "main" street area. Part of this district (fronting Miner Street) is included in the National Historic District. Improvements to property within the historic district are subject to review by the local Historic Preservation Review Commission and therefore should be sensitive to the historic context.





## 2 - EAST END NEIGHBORHOOD

Just east of the Historic District is a well-established residential neighborhood. There is opportunity to better engage Clear Creek in this area north of Colorado Boulevard, as well as the potential for daylighting Soda Creek south of Miner Street. New development should be sensitive to the historic residential character.

A **minor gateway** is located at the intersection of Miner Street and Soda Creek Road. This gateway opportunity includes the potential for a small pocket park and signage directing visitors to Downtown and the East End from Indian Hot Springs.

## 3 - EAST END CULTURAL CENTER

Starting at the Visitor Center, and including Courtney Riley Cooper Park, the Argo Mill, Safeway and the Post Office is the East End Cultural Center. This area is seen as a major cultural destination and “node” within the East End. The Argo Mill has major potential for redevelopment and heritage tourism. The new design and expansion of CRC Park will also help enliven this area. The East End Cultural Center is envisioned as becoming the major “destination” within the East End. It is within walking distance from both downtown and the rest of the East End.

A **major gateway** is located at the intersection of Miner Street and Colorado Boulevard in front of the Visitor’s Center. This prominent intersection is envisioned as acting as the western gateway to the East End with prominent signage, landscaping, and a small plaza feature.





#### 4 - MIXED USE CORRIDOR

This area includes Colorado Boulevard and Miner Streets between Clear Creek and I-70. It includes a mixture of uses such as: lodging, retail, restaurants, and residential. These uses are appropriate for the corridor, but many of the buildings are deteriorating and past their useful life. As properties redevelop, they should follow the guidelines and goals within this plan to create a more pedestrian-friendly atmosphere. New development should also engage the creek to the north and sensitively transition to the residential neighborhood along Miner Street.

#### 5 - GATEWAY COMMERCIAL CENTER

The area near Exit 241 includes mainly commercial uses. This area could benefit from more highway-oriented businesses that also contribute to an active street life along Colorado Boulevard, such as fast casual restaurants or a local brewery. A hotel may also be an appropriate use for this area, given the highway visibility. Beautification of the roundabout and surrounding landscaping is desired to help create a defined “gateway” into the East End.

#### 6 - RECREATION CENTER

New uses that compliment the ball fields and amphitheater are envisioned in this area. Development in this location has great visibility from the interstate, which could help with branding Idaho Springs as a recreation destination. Examples of facility types that might locate here include: an indoor climbing gym or recreation facility, a BMX bike course or mountain bike practice course, sports-related facility (basketball courts, batting cages, etc.), or other similar facilities.



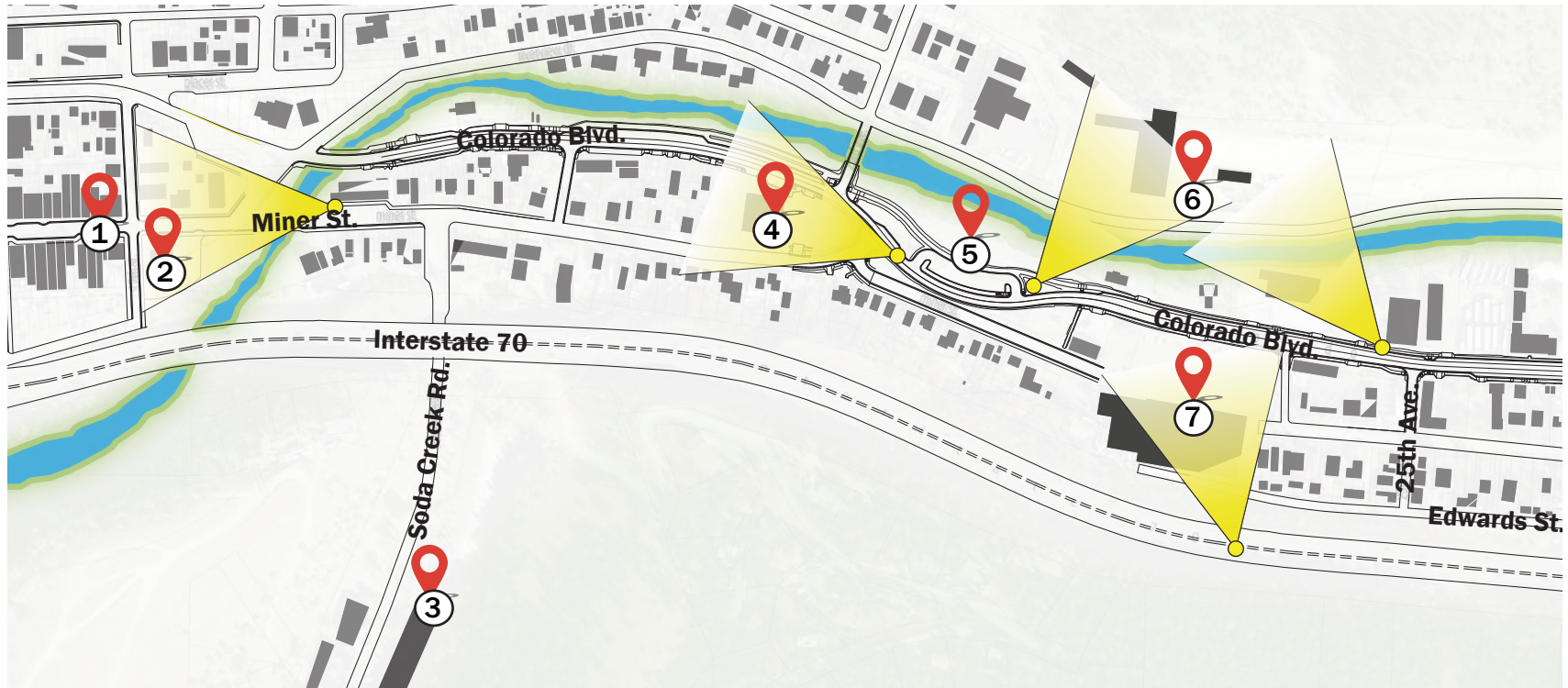


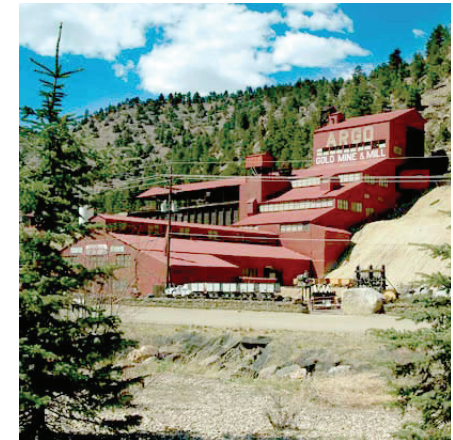
Figure 3 - Destinations & View Corridors

## DESTINATIONS AND VIEW CORRIDORS

There are multiple destinations in the East End. Most of the community destinations are located in the “East End Cultural Center” area. These destinations should be promoted with clear signage and wayfinding. They are:

- 1) Historic Downtown
- 2) City Hall
- 3) Indian Hot Springs
- 4) Visitor’s Center
- 5) CRC Park
- 6) Argo Mill
- 7) Safeway
- 8) Ball Fields

Many businesses are also considered destinations, but because some businesses change over time, this diagram focuses on highlighting more tourist and community-focused destinations.



Argo Mill





Protecting view corridors is an important consideration. Views up the gulch (and river, where feasible) as you come into Idaho Springs from Exit 241 is desired. There are also future recreation opportunities and “look outs” from atop the mountain above Idaho Springs that will be important to highlight and preserve. Other significant view corridors include protecting views of the Argo Mill historic structure. This is an iconic structure seen from the interstate, but also as new development comes into the East End, building heights should be considered to protect views of the structure along Colorado Boulevard in key locations as well. Another important view is of Miner Street and the historic downtown.



Clear Creek and CRC Park



View from Argo Mill waste rock pile



Visitor's Center



Historic Downtown

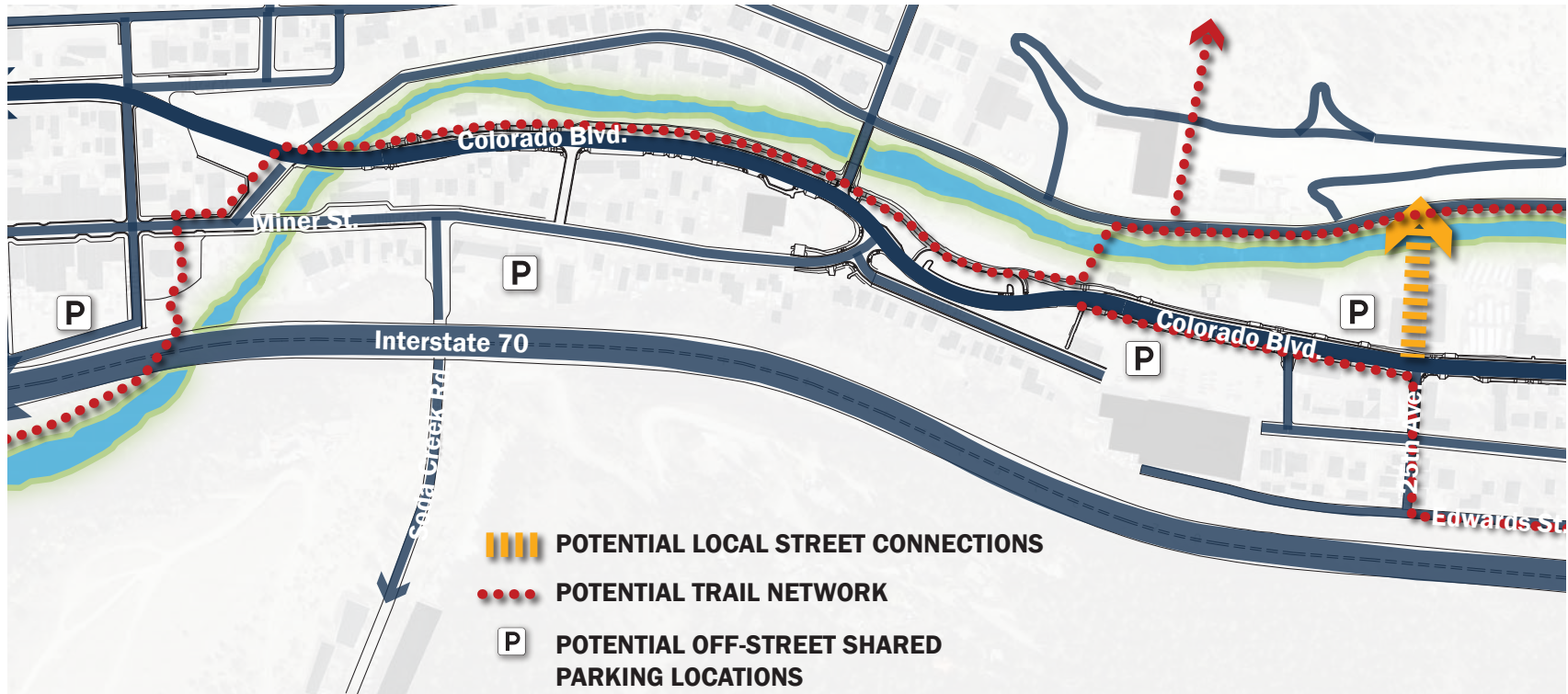


Figure 4 - Connectivity

## CONNECTIVITY, PARKING, AND TRANSIT

Currently, Colorado Boulevard is the primary street within the East End and very few local streets intersect with it. As such, it struggles to meet the needs of the community. Adding in new local street connections are viewed as a key catalyst for East End redevelopment opportunities. New streets would:

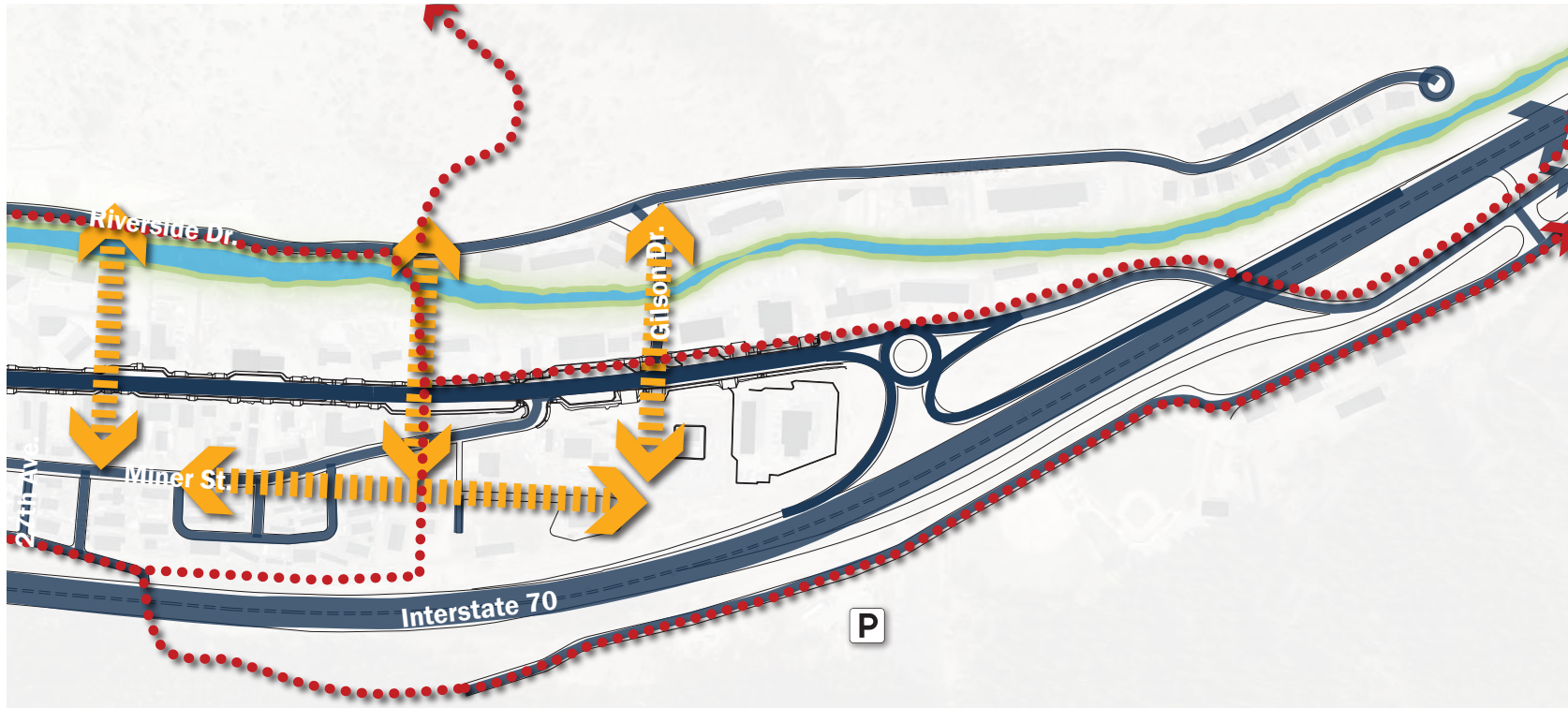
- 1) Take pressure off of Colorado Boulevard (decrease congestion),
- 2) Increase circulation and access for all users (walkable street grid),
- 3) Provide more frequent crossings at regularly spaced intersections,
- 4) Offer new opportunities for on-street parking along side streets.

Potential locations for off-street shared parking facilities are shown in Figure 5. These could be built in conjunction with a new development, and serve both the development and the larger public needs. Figure 5 also shows potential trail connections so the East End connects into the regional trail network. Finally, a regional transit (Bustang) stop is envisioned for downtown, which would also serve the East End. The stop would be located via a slip ramp off I-70.



Colorado Boulevard struggles to meet the needs of the community.





*Adding in new streets would take pressure of of Colorado Blvd., increase circulation and access for all users, provide more frequent crossings, and add opportunities for on-street parking.*



*Trail connections would connect the East End to the regional trail network.*



Figure 5 - Parks & Open Space

## PARKS AND OPEN SPACE

Having parks and open space within walking distance from a person's home or work is desirable and essential for creating "community." This plan envisions a series of public or semi-public spaces throughout the corridor as a way to provide access to nature and gathering spaces. Each should be connected to one another within the circulation system (see previous diagram).

Existing and proposed parks/open space are shown above. The proposed parks/plazas are not exact locations. They are general areas where open space is desired to help activate Colorado Boulevard, Clear Creek, and the surrounding neighborhoods. The circles surrounding each open space represent a one-eighth mile radius, or approximately a two and a half minute walk.

### 1 - WATERWHEEL PARK (EXISTING)

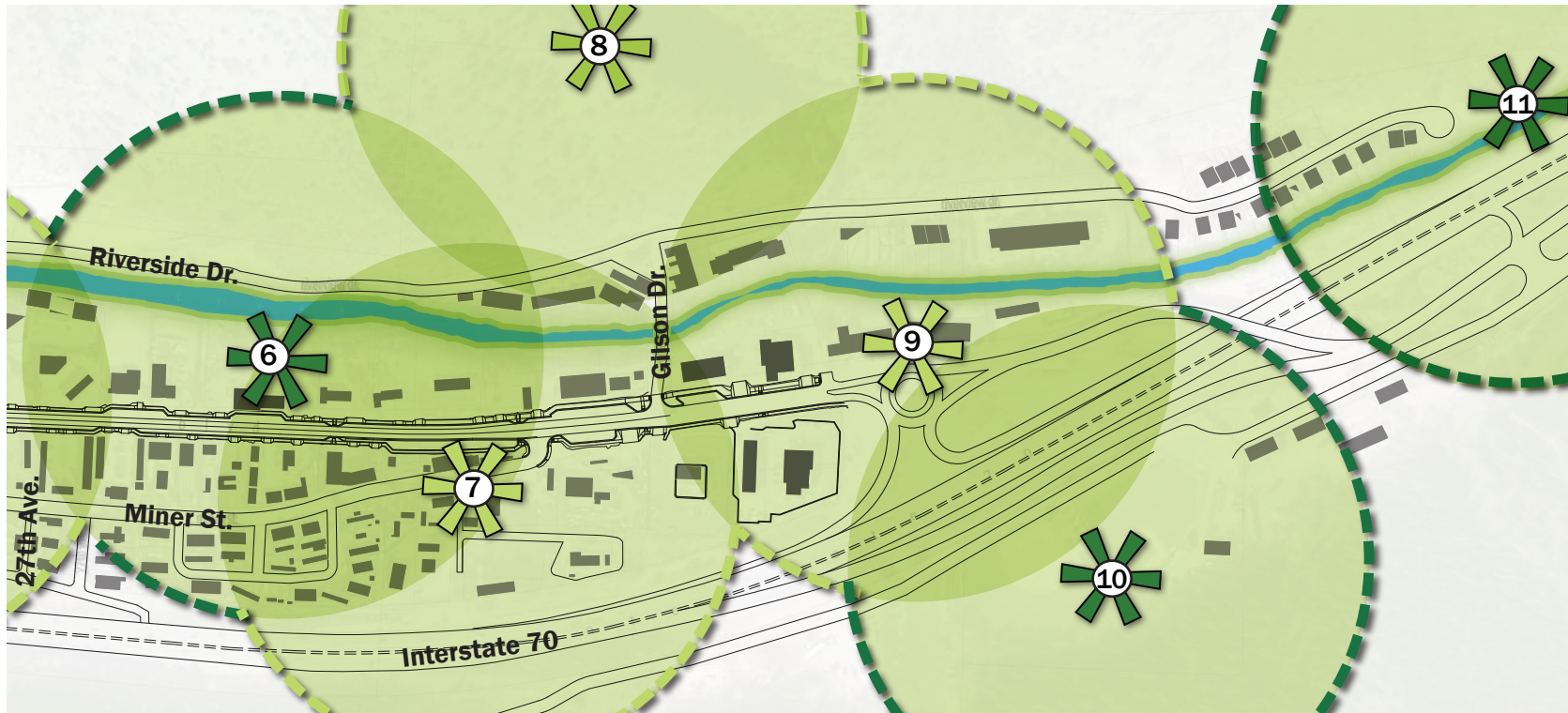
This small park tucked south of I-70 along the creek can be seen from the interstate with its iconic waterwheel and waterfall. It is located along the regional trail. Signage directing people to and from this park is important.

### 2 - SODA CREEK POCKET PARK (PROPOSED)

This small park is envisioned to serve as a "gateway" to the hot springs as well as a passive, natural area for residents to enjoy. The small park/plaza could be incorporated into a development on the site of the former lumber yard.







### 3 - COURTNEY RILEY-COOPER PARK (EXISTING)

The CRC Park shall serve as the major gathering space for the East End. It will be expanded with the redesign of Colorado Boulevard to include more amenities and space for events, as well as provide a trail connection for the greenway. The statue of Steve Canyon should be housed in the expanded park.

### 4 - ARGO MILL PLAZA (PROPOSED)

A major concept for the future of the Argo Mill is to engage the creek and connect to Colorado Boulevard and the East End seamlessly. This plaza on the creek would include a pedestrian bridge that connects to the south side and into the larger corridor.

### 5 - FESTIVAL STREET (PROPOSED)

A flexible “street” that could function as access to properties north of Colorado Boulevard (and perhaps a parking structure), but could also function as a “plaza” for special events, is envisioned in this area. The concept involves a terminus pedestrian bridge across Clear Creek that connects to the proposed Argo Mill Plaza (refer to Chapter 4 for more information on the festival street).



3



## 3 - URBAN DESIGN FRAMEWORK





### 6 - SMOKIN' YARDS PICNIC AREA (EXISTING)

The existing picnic area associated with Smokin' Yards BBQ should be continued, as it provides one of the only existing connections to the creek from Colorado Boulevard. This area could be upgraded with accessible ramps to provide better access to the space and creek, as well as a hardscaped plaza-like feature. Programming could be expanded as well, to include opportunities for small events, such as weddings or live music performances.

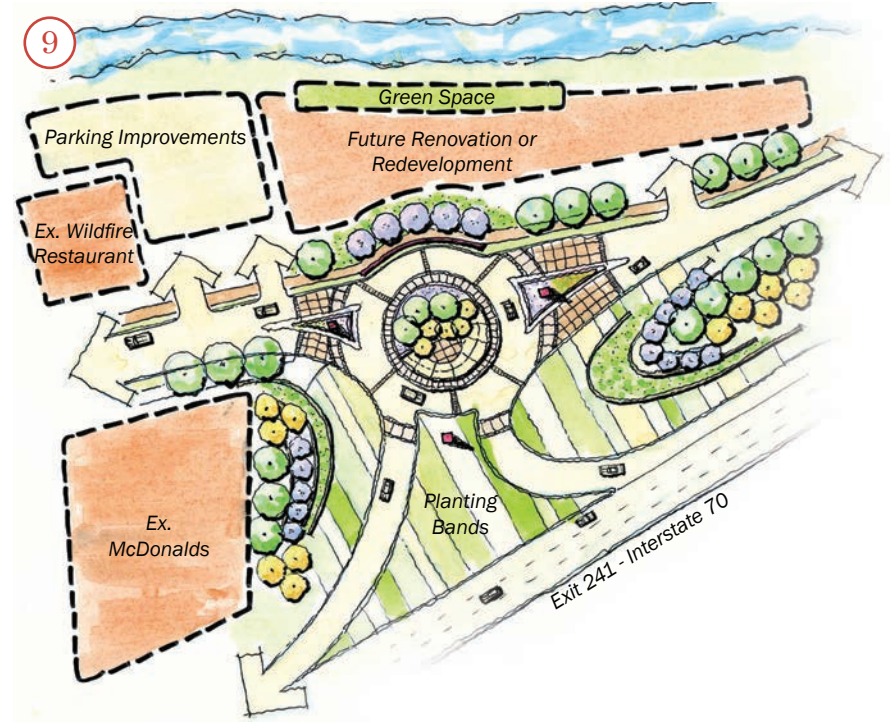
### 7 - NEIGHBORHOOD PARK (PROPOSED)

With the proposal to reconfigure the street grid on the south side of Colorado Boulevard to enhance connectivity comes an opportunity to create a triangular-shaped public park that could serve the neighborhood and act as a trail connection. This park could be fronted on all three sides, allowing it to be activated and used by multiple businesses and residents along the corridor.

### 8 - CITY HIKING AND BIKING TRAILS (PROPOSED)

The city-owned open space is 400+ acres of hillside above the East End. This action plan includes recommendations for the city to formalize the existing trail network in order to start marketing Idaho Springs as a hiking and biking destination in the Front Range. This could greatly influence economic development in the East End by providing a major public amenity and destination for residents, tourists and Front Range residents alike.





## 9 - GATEWAY PARK AND BEAUTIFICATION (PROPOSED)

Beautification of the new roundabout and areas surrounding it is recommended in order to create a sense of arrival and to serve as a “gateway” to the East End and Idaho Springs from I-70. This could include enhanced landscaping, public art, and signage. Seasonal changes could be incorporated as well, such as hosting a large Christmas tree in the center of the roundabout, as has been started by the city in 2016.

Also envisioned for this area is a small, semi-public open space as part of the 6 & 40 Motel site when it redevelops. It currently includes a pleasant creek-side picnic area. Views of the gulch to the north could be framed by new buildings and open space.

## 10 - SHELLY/QUINN BASEBALL FIELDS (EXISTING)

This plan envisions building upon the existing ball fields with additional recreation opportunities (refer to Chapter 5 for more information on this site). This park should continue to be used for youth sports, tournaments, and festivals. New recreational uses should encourage more use of the fields by complementing them.



## 11 - SKATE PARK (EXISTING)

The skate park is tucked behind the townhomes at the end of Riverside Drive. The park could benefit from enhancements such as using more permanent structures, to become more of a draw for the youth in Idaho Springs. Safe connections to the park are important, such as adding curb, gutter, and sidewalks to Riverside Drive, as well as incorporating it into the future greenway trail connection. Lighting should be added along Riverside Drive and at the park for increased safety.



## 3 - URBAN DESIGN FRAMEWORK



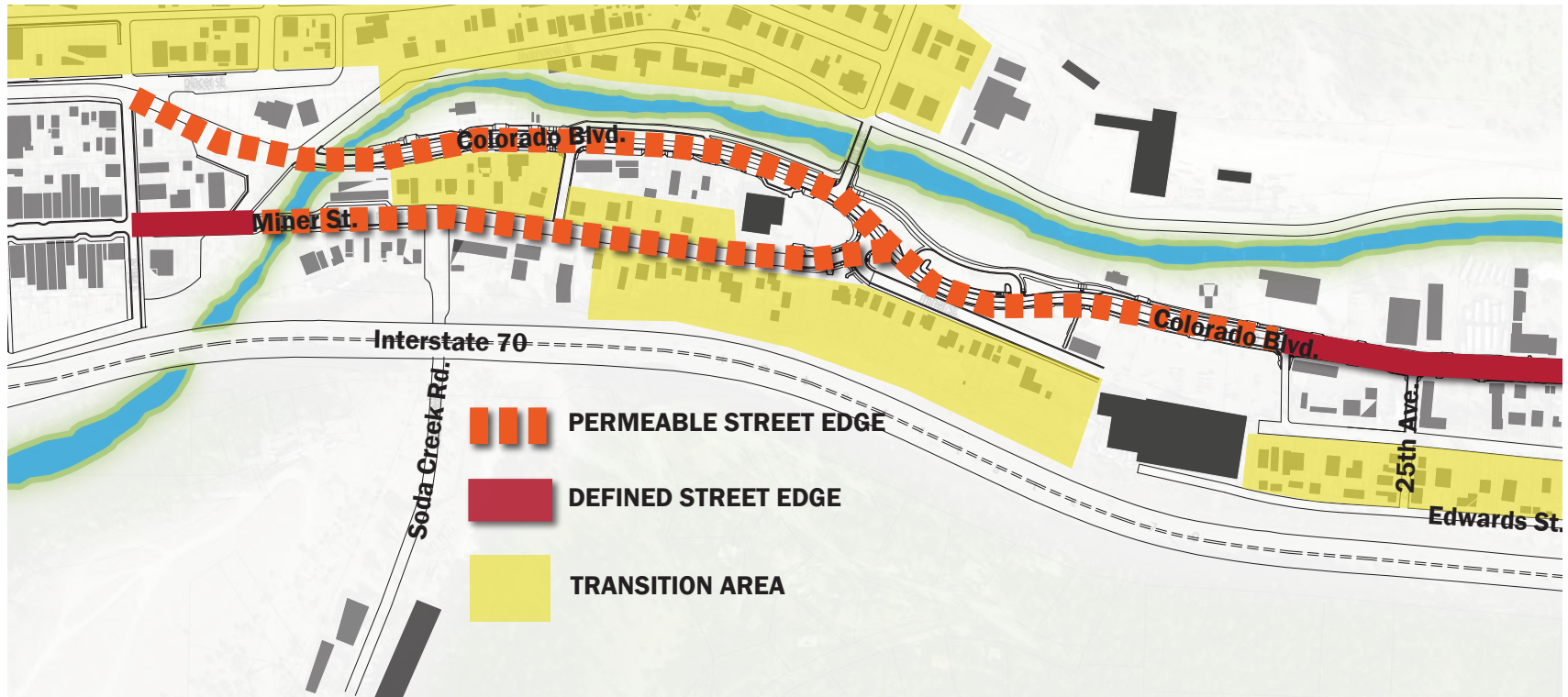


Figure 6 - Edges and Transitions

## EDGES AND TRANSITIONS

Edges refer to how existing and new buildings meet the street and contribute to a sense of enclosure for the street. Edges are represented as:

- 1) **A defined street edge** where buildings orient to and are built close to the street for the majority of a property's frontage to help frame the street as a "room" and to create a pleasant walking experience. Parking lots along the street would NOT be a defined edge.
- 2) **A permeable street edge** where there is more space between buildings and where a building does not necessarily orient to the street for the majority of a property's frontage. A residential street is an example of a permeable edge, where the building orients to the street, but is set back and has more space on either side of it.

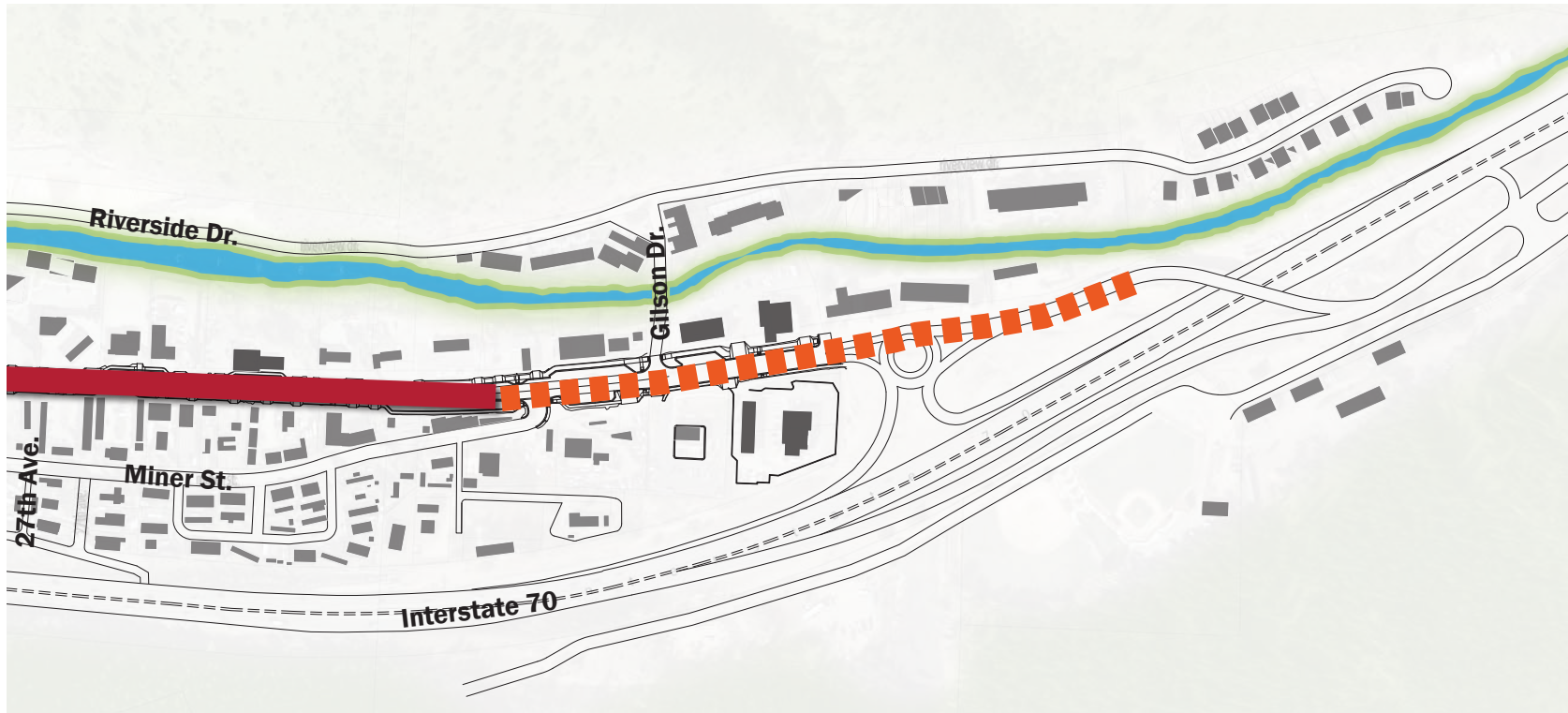
These two conditions are shown primarily along Colorado Boulevard, because it is the primary spine through the East End into downtown. Edges are also shown along Miner Street from downtown to where it intersects Colorado. All residential streets should be considered to be permeable. Refer to Appendix A: East End Overlay District for specific requirements regarding street edge frontages..



A permeable edge includes buildings set back from the sidewalk and with more space between buildings.

EAST END ACTION PLAN





Transitions refer to how new development relates to adjacent, sensitive areas. The areas shaded in yellow are representative of the primarily stable single family residential neighborhoods in the East End and therefore new development should transition sensitively toward them with decreased mass and scale, increased setbacks, and similar architectural features and uses.

The East End Overlay District (Appendix A, Section 46-7) offers specific standards that manifest the concept of defined and permeable edges and transitions to single-family residential neighborhoods.



*The block shown here transitions from a commercial street to a residential street by placing a residential building between the parking lot and the residential street.*



*New development should transition sensitively to single family residential uses with decreased mass and scale, increased setbacks, and similar architectural features and uses.*

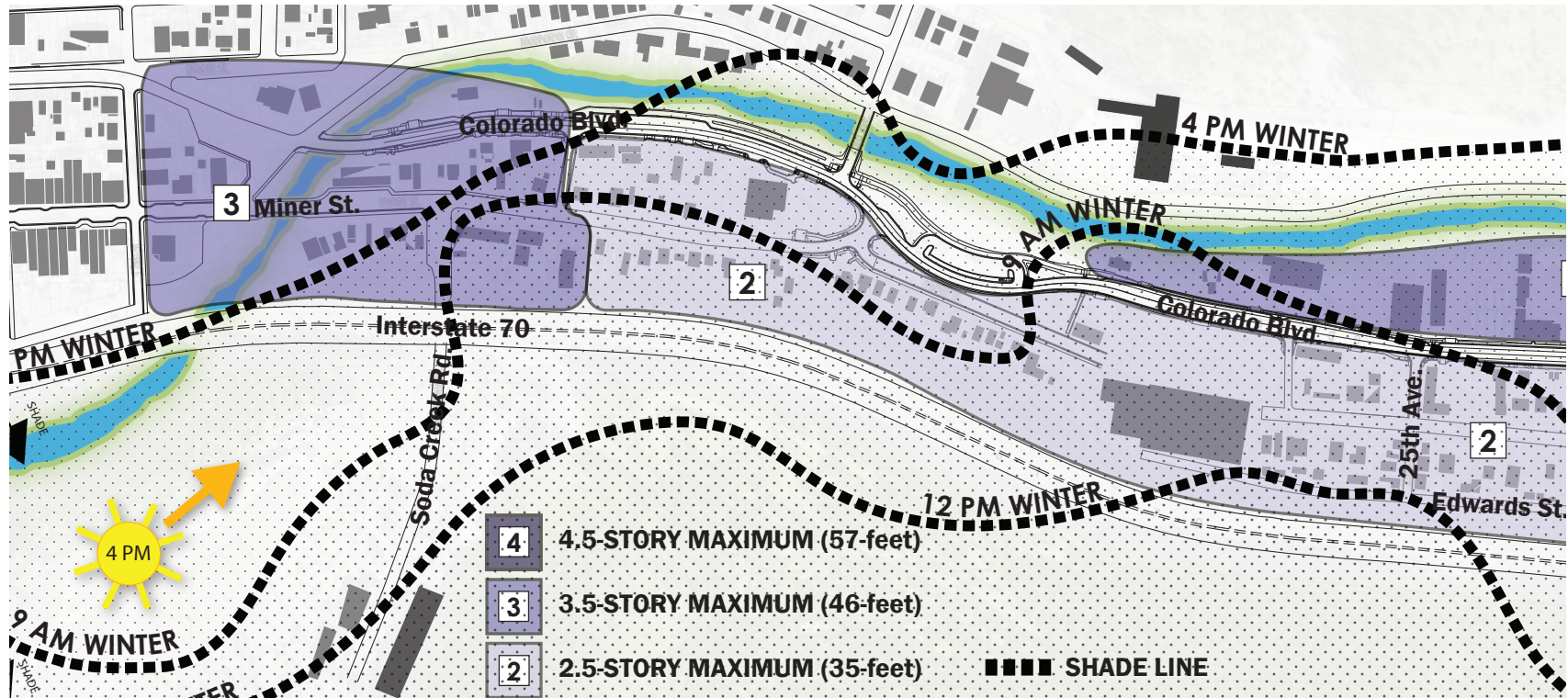


Figure 7 - Sun Access & Building Height

## SUN ACCESS AND BUILDING HEIGHT

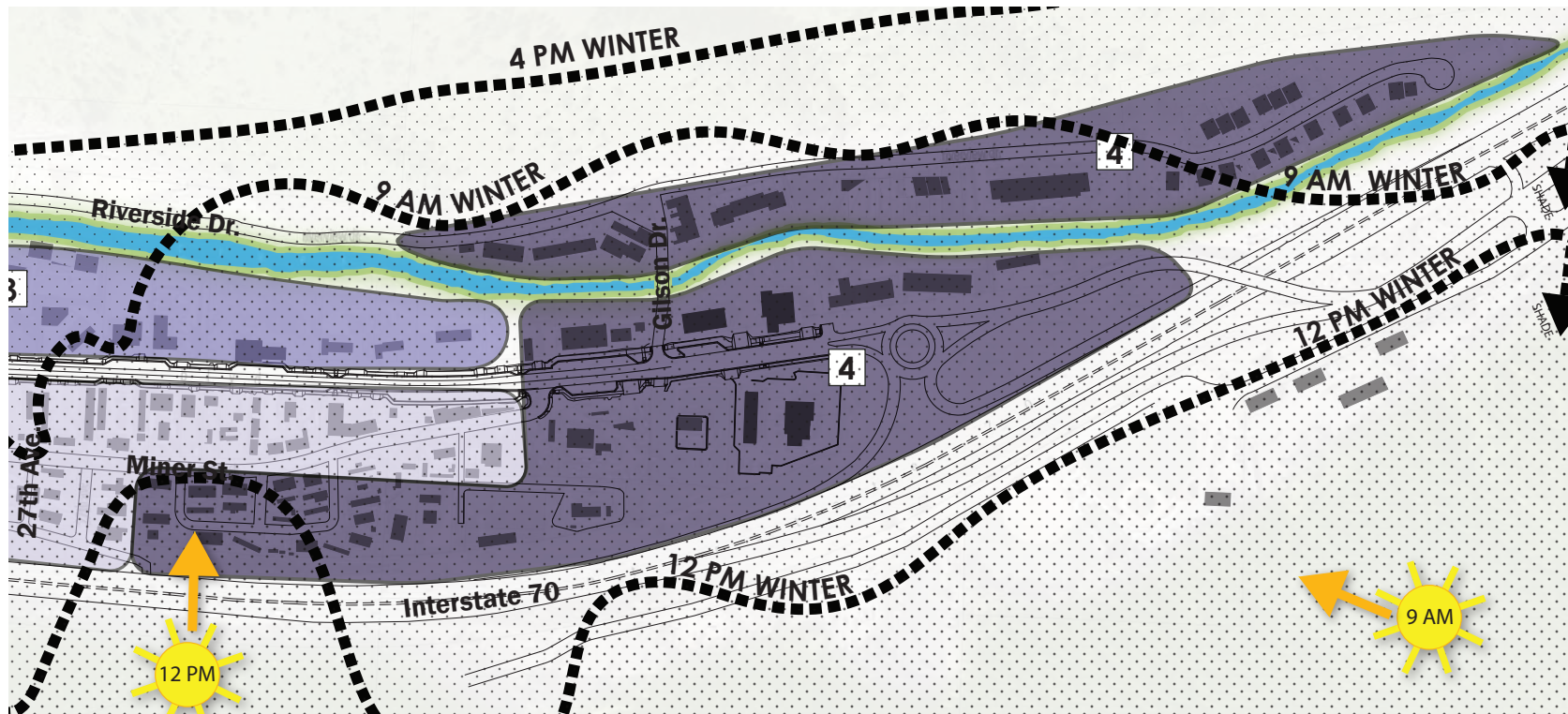
Providing ample sunlight to existing and new development is also important. Being located in a valley makes access to sun more limited. Therefore, new development should be sensitive to this concern. The worst-case sun access scenario (winter solstice) is mapped on the above diagram to show how much of the site is in natural shade from the mountains in the winter. This sun study mapping exercise determined the proposed future maximum building heights.

Future maximum building heights are reflected in the overlay zone associated with this plan. They are established with the following in mind:

**2-Story Maximum** - the 2-story maximum area includes properties on the south side of Colorado Boulevard between 20th Avenue and the proposed "Argo Avenue". This is to ensure that the sun can permeate onto Colorado Boulevard for snow and ice melt as well as to give properties north of Colorado Boulevard access to sun as well.

**3-Story Maximum** - the 3-story maximum area includes properties on the north side of Colorado Boulevard as well as those closest to downtown. Allowing taller buildings on the north side of Colorado is due to the fact that



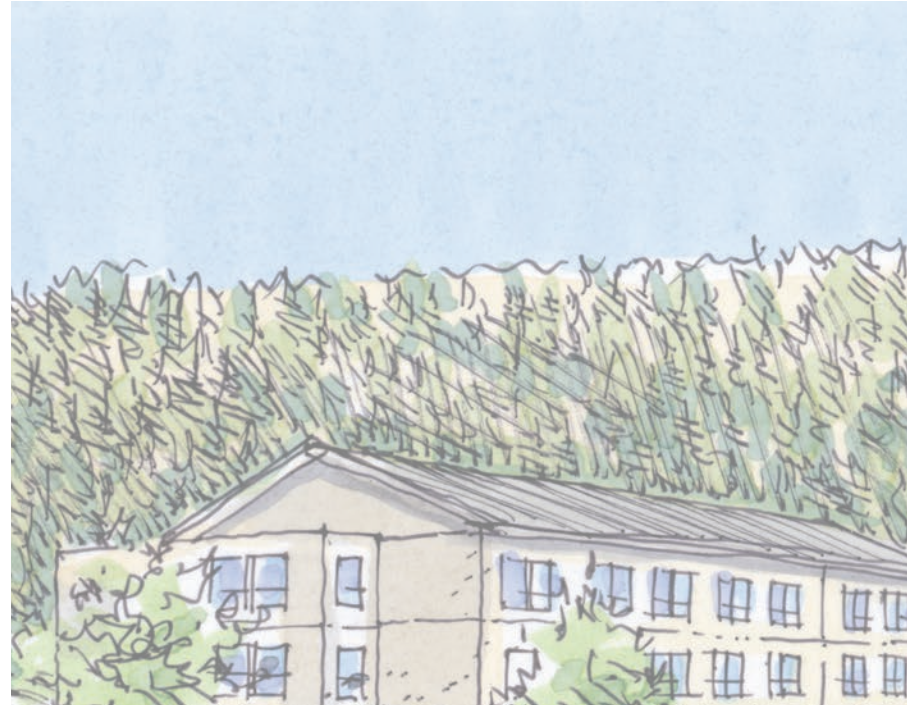
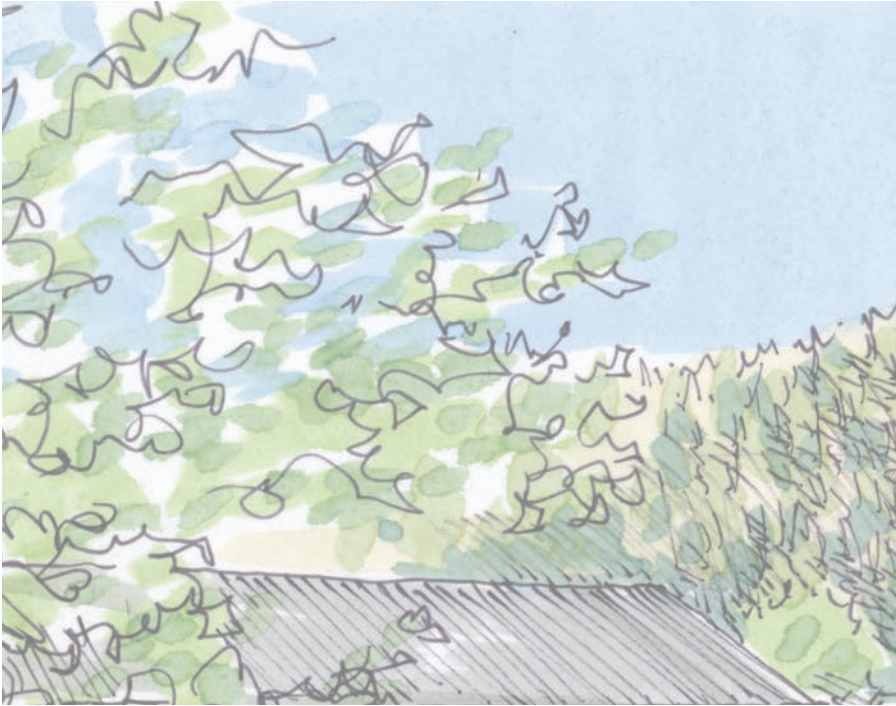


these properties are only shading themselves, and not significant public spaces such as streets or parks. These properties should be sensitive to their neighbors to the east and west as well, allowing as much sun access as feasible with new development.

**4-Story Maximum** - the area closest to Exit 241 (East End Commercial Center district) is designated for 4-stories. This is due to the fact that Colorado Boulevard has a wider right-of-way in this area (100-feet) as well as more flexible set backs for buildings. This means that properties in this area have more access to sun, and taller buildings will not adversely affect neighbors as much as other locations along the corridor. Additionally, this area is envisioned as a gateway with high visibility from the interstate, so taller buildings are more appropriate in this context.

It is important to note that the transition areas highlighted in Figure 6 apply to any of these height zones. New development, regardless of maximum height, should “step down” in scale toward stable single family neighborhoods to protect the overall scale and character of the East End.







# 4

## Streets and Public Space

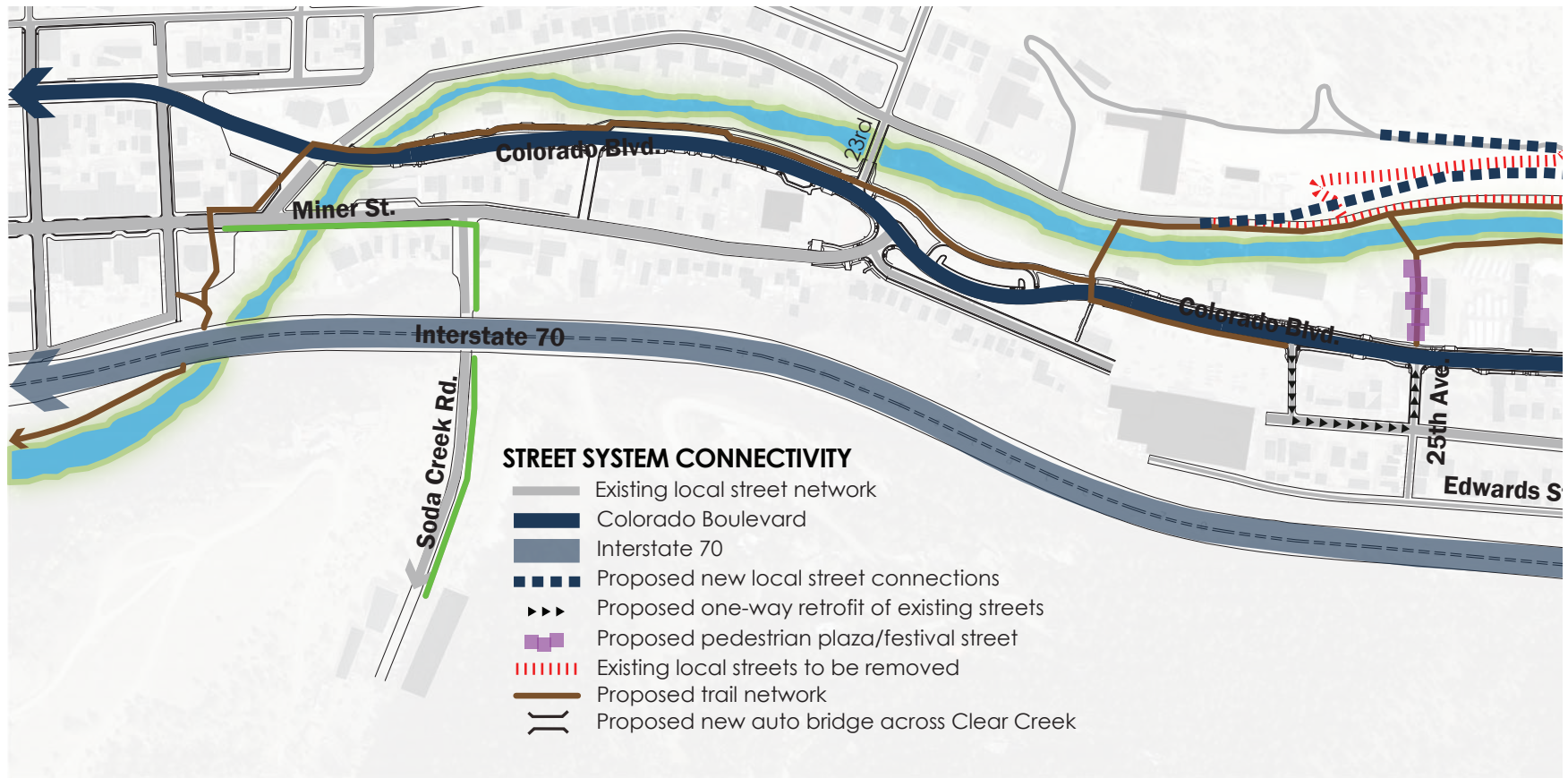


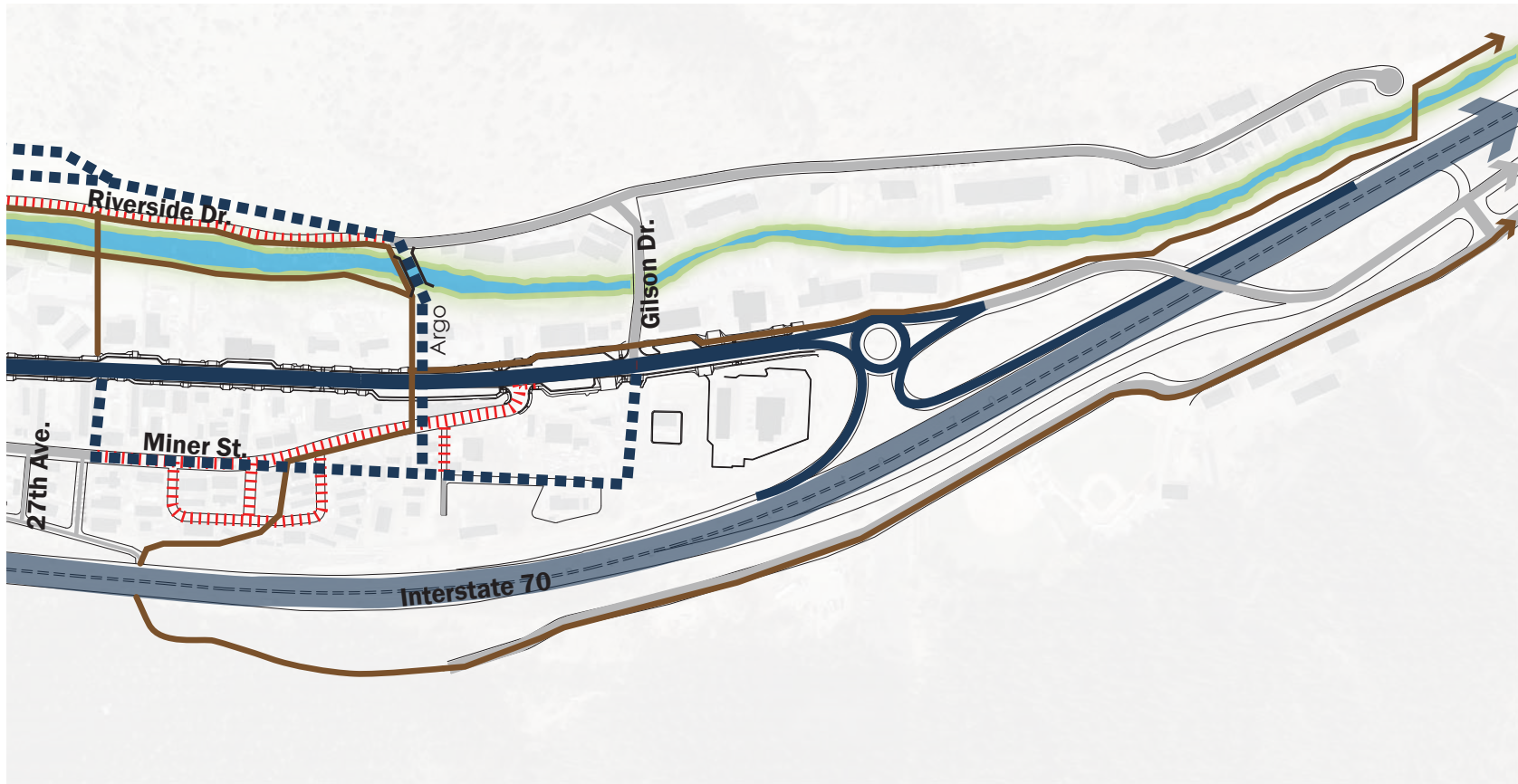
Figure 8 - Street System Connectivity

## STREET SYSTEM CONNECTIVITY

### COLORADO BOULEVARD

Colorado Boulevard is the main east-west street through Idaho Springs. Formerly Highway 4/60, this corridor serves as a truck and bus route, provides a direct connection to I-70 at exit 241, and for many visitors to Idaho Springs, serves as the front door to the community. However, the character is currently auto-oriented in terms of both roadway design and land use, due in part to deep lots fronting the roadway and a lack of an intersecting local street grid. As part of devolution from CDOT, Colorado Boulevard is currently being reconstructed as a “complete street” with sidewalks, curb-and-gutter and on-street parking to be added. Near the Visitor’s Center, the street alignment will shift slightly to create an expanded Courtney-Ryley-Cooper (CRC) Park and a new entry into Downtown along Miner Street.





## LOCAL STREET SYSTEM

To improve connectivity within the East End, three future local streets are proposed to be built between Colorado Boulevard and a realigned Miner Street. The intersecting streets will be spaced 575'-675' apart to create multiple, smaller blocks that will allow for redevelopment of sites accessible via side streets. The enhanced street grid will improve circulation options for cars and bikes, and will greatly enhance the walkability of the East End as all new streets will include sidewalks and streetscape elements. With future extension of the local street system across Clear Creek, the grid is designed to work with proposed redevelopment of the Argo site, a relocated Riverside Drive, and completion of the Clear Creek Greenway through Idaho Springs.

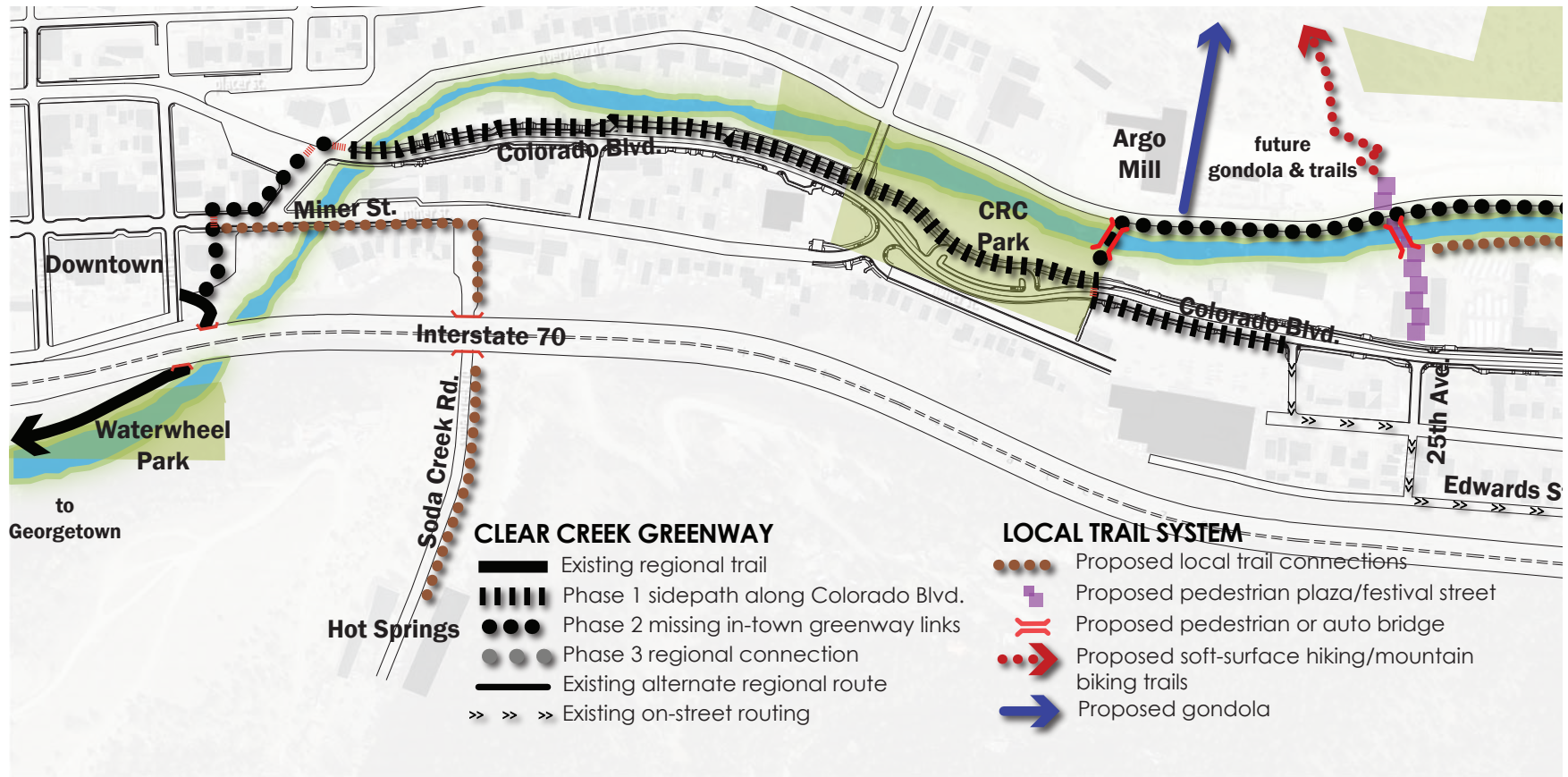
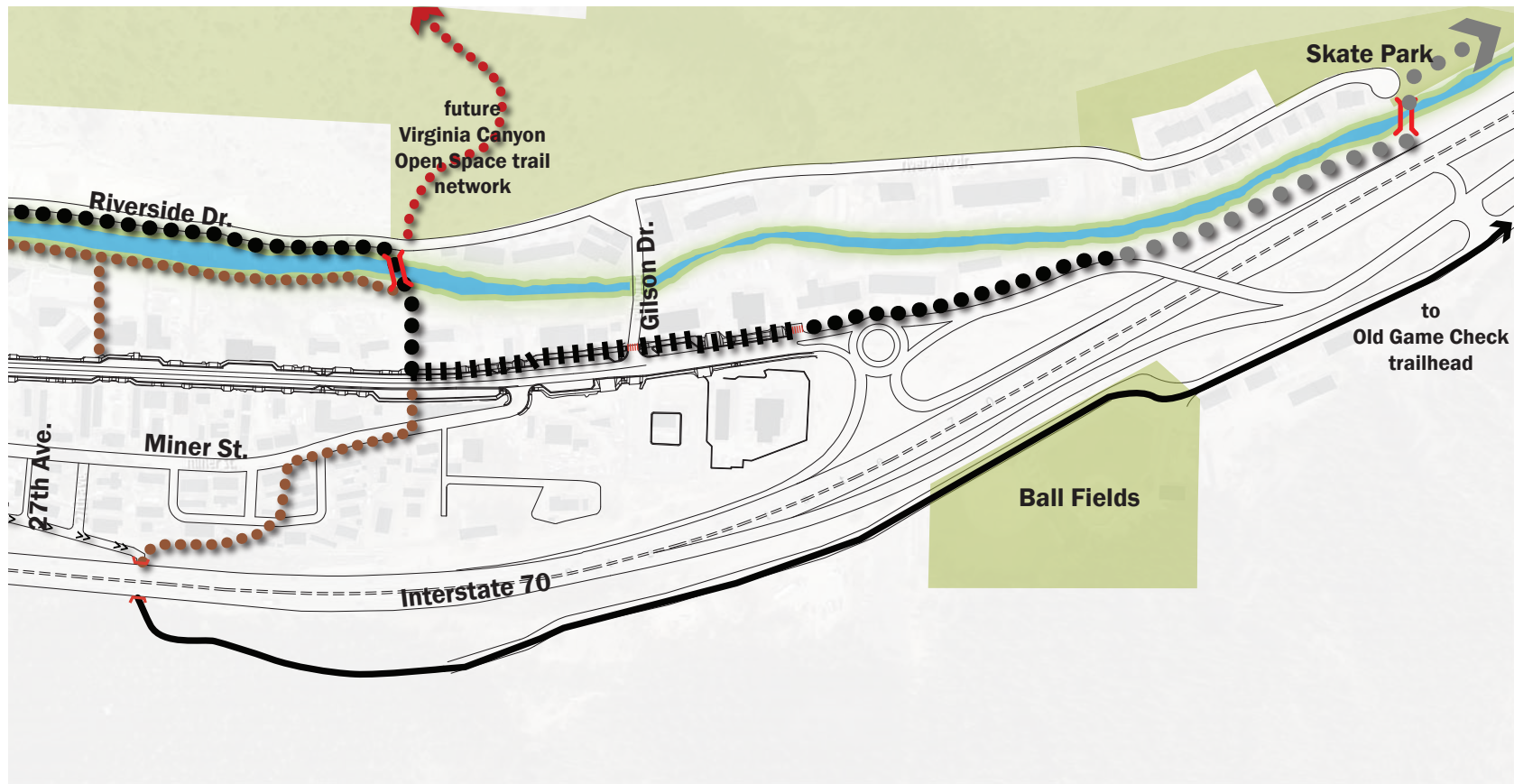


Figure 9 - Clear Creek Greenway

## TRAILS SYSTEM **CLEAR CREEK GREENWAY**

The Clear Creek Greenway, spanning 36 miles from the Jefferson County border to the Eisenhower Tunnel, is part of an even longer recreational trail corridor that will eventually link the Platte River Greenway in Denver to the Continental Divide National Scenic Trail. Through Idaho Springs, the project is being implemented in phases in conjunction with reconstruction of Colorado Boulevard and award of grant funding from Great Outdoors Colorado (GOCO) and Colorado Parks and Wildlife (CPW). When completed, the primary multi-use greenway trail will follow the banks of the Clear Creek, with an alternate route located south of I-70 on the existing Scott Lancaster Trail.

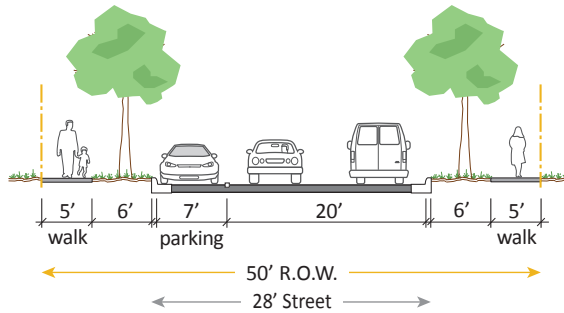




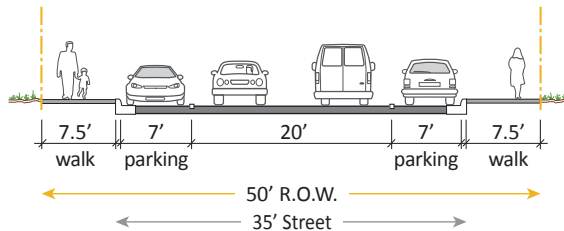
## LOCAL TRAIL SYSTEM

To supplement the regional greenway routing through the East End of Idaho Springs, a system of local connector trails will be developed to provide north-south mobility for bicyclists and pedestrians, enhance access to the Argo, Hot Springs, ball fields, skatepark and other destinations, and create a system of pathway loop options that will enhance recreational opportunities for Idaho Springs residents and visitors. The long-term vision also includes a system of stacked-loop hiking and mountain biking trails, and a supporting gondola line, that will be developed on Argo and City-owned property in Rosa Gulch and the surrounding mountainside. The intent is to attract day-use recreational visitors and provide an economic boost to the community.

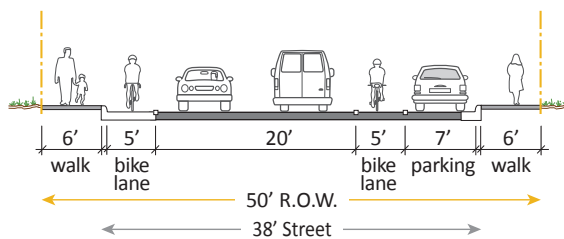
50' R.O.W.  
Option 1  
Typical  
Local Street



Option 2  
Dual Parking



Option 3  
Bicycle Lanes



**Standards for Narrow Local Streets:**

- Secure 50' right-of-way widths to accommodate on-street parking on at least one side and sidewalks on both sides of all new local streets.
- Provide detached sidewalks wherever possible.
- Clear zone sidewalk width (without obstructions) is 5' minimum. Signs and street furnishings must not be located or overhang within this 5' min. clear zone.
- Where sidewalks must be attached due to topography or other limitations, the minimum width adjacent to back-of-curb is 6', with 7' min. preferred.
- The vehicular travel way should be narrow to encourage slow travel speeds. A 20' clear width is recommended for fire and emergency access.
- Overall street widths will vary depending on desired uses. The recommended 50' typical right-of-way may be varied to accommodate on-street bicycle lanes or parking on both sides of the street
- Where desired for greenway/bicycle system connectivity, streets may be striped with 5' wide bicycle lanes – one-way facilities designated with paint markings on both sides of a street.
- Due to right-of-way width restrictions, an integral-pour 5' gutter pan is recommended when the bike lane is adjacent to the curb face to provide the needed minimum bicycle lane width within the narrow 50' R.O.W.

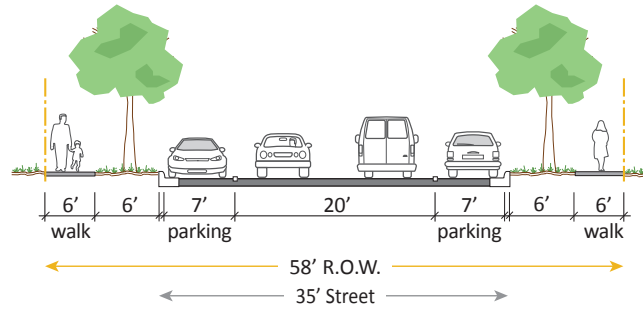
## STREET AND TRAIL SECTIONS

### COMPLETE STREETS

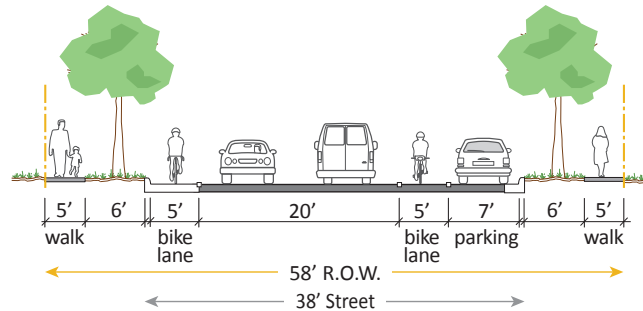
All new local streets within the East End study area will be constructed as “complete streets” and include design elements to accommodate all modes of travel. Right-of-way widths for local streets will include adequate space for elements of the pedestrian realm to encourage walking within the redeveloped East End mixed-use district. Narrow street widths and on-street parking will encourage slow travel speeds. Sidewalks will be provided on both sides of every street, detached wherever feasible to buffer pedestrians from vehicular traffic. Bikes and cars will typically share lanes where traffic volumes are low and speeds are slow. Where future street connections are desired to serve as on-street greenway linkages, consideration may be given to add striped bicycle lanes.



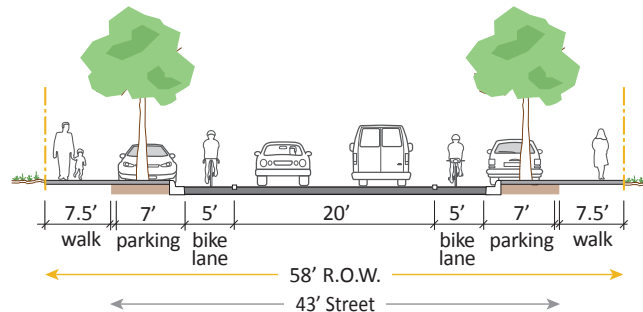
**58' R.O.W.**  
Option 4  
Dual Parking  
w/ detached  
sidewalks



Option 5  
Bicycle Lanes  
w/ detached  
sidewalks



Option 6  
Bike Lanes  
Dual Parking  
Curb Extensions



#### Standards for Wider Local Streets:

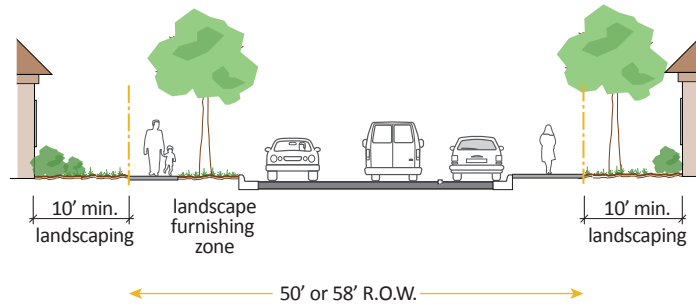
- Expand right-of-way widths to 58' to accommodate enhanced streetscape character and detached sidewalks on wider streets.
- Provide planting strips, or furnishing zones, between street and sidewalk to enhance streetscape character and provide separation between pedestrians and vehicles.
- Locate street trees, signs, utilities, mailboxes, lighting, street furniture and so forth within the furnishing zone to maintain an adjacent 5' minimum clear sidewalk width, free from obstructions.
- Providing a furnishing zone 6' in width is recommended to accommodate healthy tree growth and required 1:20 running slopes on ADA sidewalk curb ramps.
- Use of landscape planting strips is recommended within residential areas and in front of large commercial properties with large setbacks and on-site parking. Paved furnishing zones with street trees planted within sidewalk tree wells is recommended to provide hardscape adjacent to on-street parking within mixed-use areas with ground floor commercial. (See frontage character next page.)
- Where on-street parking is desired, consider adding curb extensions, or bulb-outs, within the parking lanes to visually narrow street widths and provide space for street tree plantings and pedestrian amenities at street corners.
- Extend bulb-outs the full width of the on-street parking lane, retaining adequate street width for bicycle lanes, when present.

## STREETSCAPE CHARACTER

The presence of detached sidewalks, separated from the street by a landscape buffer or furnishing zone, will greatly add to the pedestrian-friendliness of local streets within the East End. Furnishing zones at least 6' wide will provide space for utilities, healthy street tree plantings, street lighting, pedestrian amenities, snow storage and ADA-compliant curb ramps. To provide this space where streets are desired to include dual-sided on-street parking and/or bicycle lanes, increased right-of-way widths, in combination with small private property setbacks, should be implemented. Since the East End Action Plan will be realized over time, various options for complete street cross-sections are thus provided to work with a variety of future private sector redevelopment scenarios.

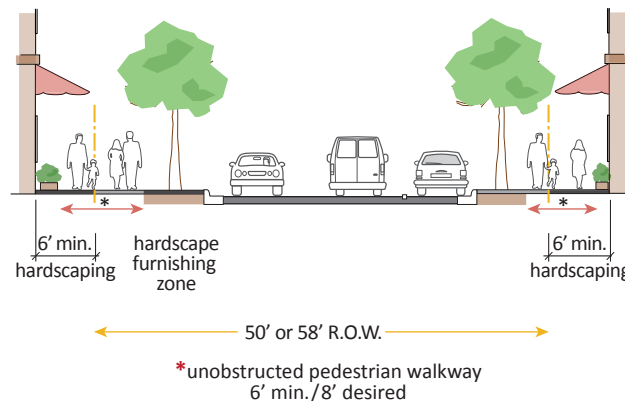
## Residential Frontage

Landscape treatment in front of residences



## Commercial Frontage

Hardscape treatment in front of first floor commercial uses



### Standards for Frontages:

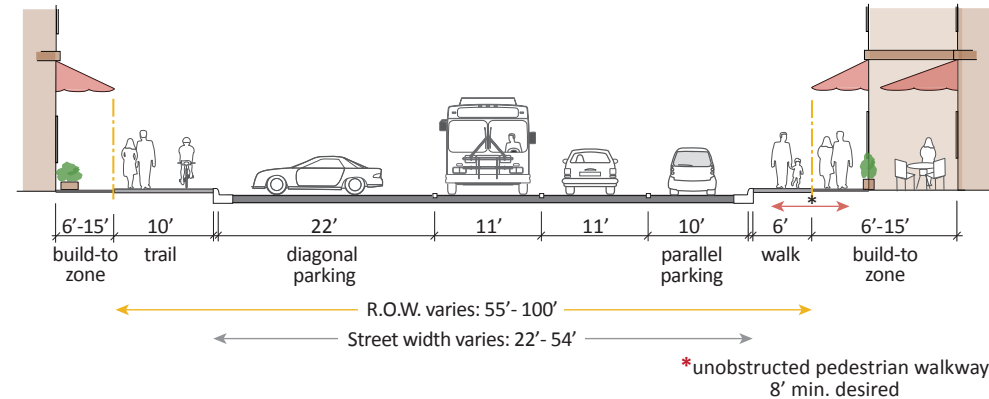
- Use build-to zones to create small setbacks, provide enclosure for the street, and encourage infill development consistent with neighborhood character. (See proposed East End Overlay District.)
- Space street trees 25'-35' on center to visually narrow the street corridor and provide a canopy of shade.
- Implement appropriate furnishing zone and setback design treatments per adjacent ground floor land use.
- In areas of residential use, encourage small landscaped front yards, paired with detached sidewalks and landscaped furnishing zones with street tree plantings.
- In areas with ground floor commercial uses, plant street trees in tree wells located next to the curb. Where plant materials are desired next to buildings, use pots or planters that do not allow encroachment upon the through walkway width of the sidewalk.
- Where commercial uses, provide continuous hardscape treatment from back-of-curb to face-of-building. Within this area, provide an unobstructed through pedestrian walkway width of 6' min./8' encouraged. Locate all pedestrian amenities, planters, signage and other obstructions outside of the minimum through walkway width.
- Maintain an 8' min. vertical clearance above all sidewalks, and a 2' min. horizontal shy distance from the through walkway to all buildings and walls.
- Provide visual interest on building facades to support a pedestrian-friendly district. This includes transparent doors and windows, awnings that extend over the sidewalk, and pedestrian-scale signing and lighting.

## FRONTAGE CHARACTER

Pedestrian-friendly frontages within the East End will be compatible with surrounding neighborhoods and address building setback distances, ground plane design, and vertical building facade elements. Use of minimum setbacks or build-to zones will be encouraged within the East End. Setback design treatments on private property will vary depending on first floor land use – landscaping will be used in front of residential uses, and sidewalk widths will be extended as hardscape setbacks in corridors where mixed-use and commercial buildings face and embrace the streets. The same guidance for implementing landscape vs. hardscape treatments according to land use context will also apply to furnishing zones within the public right-of-way.



## 55'-100' R.O.W. Colorado Boulevard Reconstruction



### Standards for Street R.O.W. :

- Reconstruct Colorado Boulevard with 11' travel lanes to accommodate wider vehicles that will regularly use this truck/bus street.
- Implement parallel or diagonal parking, per the approved Colorado Boulevard Phase III reconstruction plans.
- Due to R.O.W. constraints, 6' sidewalks will be constructed at back-of-curb, which represents an absolute minimum standard. There will be no street tree plantings within the R.O.W., thus planting of trees on private property should be encouraged where feasible to provide some greening to the streetscape.
- Sidewalks along two sections of Colorado Boulevard will be widened to 10' to accommodate routing of the Clear Creek Greenway (see pages 11-12). As an attached sidepath facility, this represents an absolute minimum acceptable width for bicycle use. Therefore, streetscape furnishings and signage must not obstruct this 10' clear width and a 3' min. horizontal clearance must be provided from edge of trail to adjacent buildings, walls, fences, and tree plantings.
- Encourage the future consolidation of driveway accesses to minimize disruption to the adjacent trail and sidewalks and to strengthen access management for traffic flow on Colorado Boulevard.

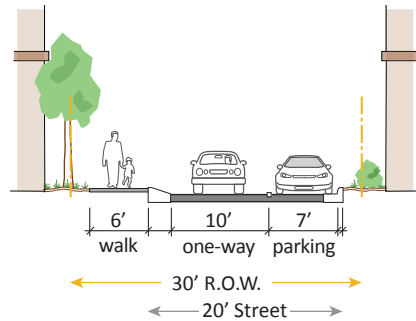
### Standards for Frontage:

- Implement build-to zones, 6' min. in width, to accommodate an unobstructed through pedestrian walkway width of 6' min./8' encouraged. (See proposed East End Overlay District.)
- Locate all pedestrian amenities, planters, signage and other obstructions outside of the minimum through walkway width.
- As discussed under local street frontages (see page 7), additional sidewalk width should be encouraged to be provided as part of site redevelopment. At minimum, provide a 2' shy distance from face-of-building to the 6' clear walkway width to allow for opening doors, window box overhangs, etc. Provide continuous hardscape treatment from back-of-curb to face-of-building.
- Where plant materials are desired next to buildings, use pots or planters that do not allow encroachment upon the through walkway width of the sidewalk. Locate all pedestrian amenities, signage and other obstructions outside of the minimum through walkway width.
- Maintain an 8' minimum vertical clearance above all sidewalks.
- Provide visual interest on building facades to support a pedestrian-friendly district. This includes transparent doors and windows, awnings that extend over the sidewalk, and pedestrian-scale signing and lighting.

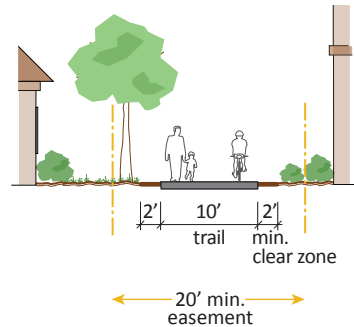
## COLORADO BOULEVARD

As part of the devolution of Highway 6/40 from CDOT, Phase III of reconstruction on this primary east-west travel route is scheduled to occur in 2017/2018. Colorado Boulevard serves a unique function and has a unique character from the smaller "local" streets within the East End District. Cross-section recommendations depict 90% design plans that have been completed for the corridor. As the primary truck/bus street, travel lanes will be wider than on local streets to accommodate larger vehicles. Plans include a mix of parallel and diagonal on-street parking, attached sidewalks, and curb extensions at crosswalk locations. Supplemental build-to zones and frontage recommendations of the East End Overlay District will further expand the streetscape and setback requirements for properties facing this important street.

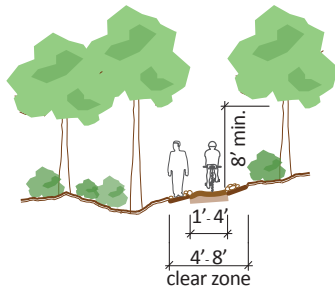
### 30' R.O.W. One-Way Street Retrofit



### Multi-Use Paths Regional Clear Creek Greenway and Local Trail Connections



### Soft-Surface Trails Low-Impact Natural Hiking and Mountain Biking Trails



## ONE-WAY STREETS

All new local streets will be constructed within 50' or wider rights-of-way to include complete street elements. However, several existing local streets are very narrow, constructed within 30' rights-of-way. Several of these are currently designated for one-way travel. Retrofit of key blocks will formalize the one-way vehicular travel patterns and add sidewalks to one side of the street for pedestrian system connectivity.

### Standards for Narrow One-Way Streets:

- Retrofit segments of 24th Avenue, 25th Avenue and Miner Street to enhance the pedestrian environment.
- Provide for on-street parking on one side of the street.
- Add an attached sidewalk, 6' min. width.
- Construct a 20' street width, with a roll-over curb adjacent to the sidewalk to ensure an adequate clear zone for fire and emergency access.
- Wherever possible, strive to maintain existing vegetation.

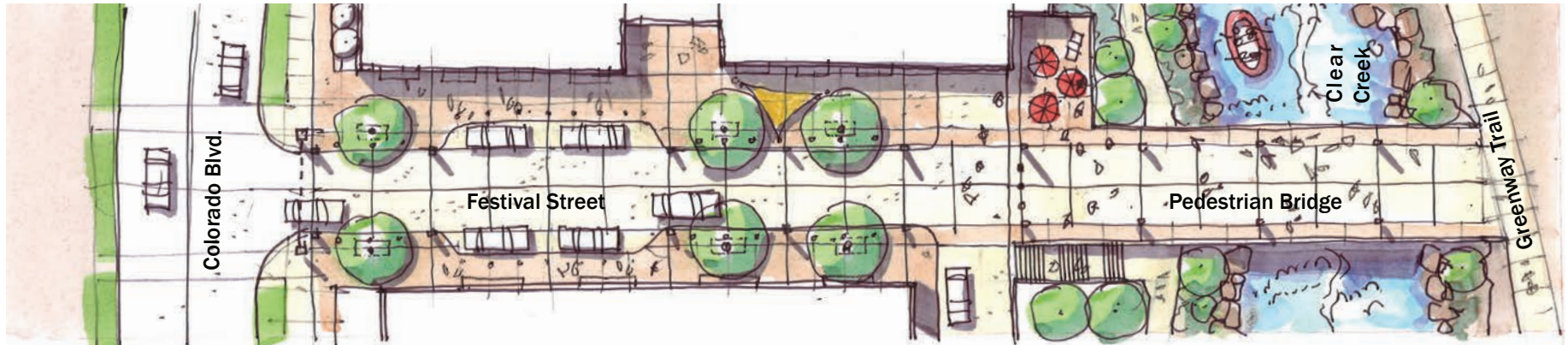
### Trail Standards:

- Construct greenway paths to be used by bicyclists to a 10' min. width, with 5% max. longitudinal grade. In areas where high multi-use is expected, consider building trails to 12' widths.
- Provide a 2' graded shoulder area.
- Maintain a 3' min. horizontal clearance from the edge of path that is free from rocks, trees, signage and other obstructions. Maintain a vertical clearance of 8' min./10' desired from all overhanging vegetation.
- Construct bridges and structures at the same width as the approaching path. Design for loadings to support necessary maintenance and emergency vehicles.
- Plan a mountainside system of soft-surface hiking and mountain biking trails that will be sustainable and cause minimal impact to the natural environment.
- Develop routes as loops to provide a variety of trail lengths and experiences.
- Construct trails that require minimal clearing, grading and construction of steps and other structures. Provide narrow trail treads. Strive to maintain an average grade of 10% or less.
- Avoid routing trails on the fall line of steep grades. Water will flow on the shortest route down a hill, and should not be channeled onto the trail tread. Instead, construct rolling contour trails with outsloping and grade reversals to force water to sheet across and off the trail, thereby minimizing soil loss. Where necessary, build bench-cut trails, add small rock or wood retaining walls, and use armoring through soft, low-lying wet areas.
- Plan interest into the trail routes. Use large rocks and trees to anchor and define the route. Use switchbacks and climbing turns to help the trail navigate directional and elevation changes. Route through choke points to create gateways and experiential views. Provide man-made structures to add technical detail, as appropriate.

## TRAIL TYPES

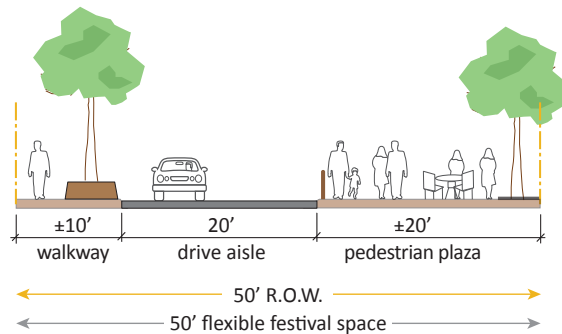
A variety of new trails will be built within the East End. All trails within town will be planned to accommodate multiple users including pedestrians, bicyclists, in-line skaters, wheelchair users, baby strollers, etc. Trail widths and grades will be designed for use by bicyclists and users with mobility challenges, following national guidelines of the American Association of State and Highway Transportation Officials (AASHTO) and the Americans with Disabilities Act (ADA). In contrast, the trail system to be developed within Rosa Gulch, leading up the ridgeline, will be rugged, twisty and natural in character. Guidelines produced by the International Mountain Biking Association (IMBA) should be used to create a stacked loop system of sustainable natural-surface trails.





concept in plan view

### 50' R.O.W. Festival Street



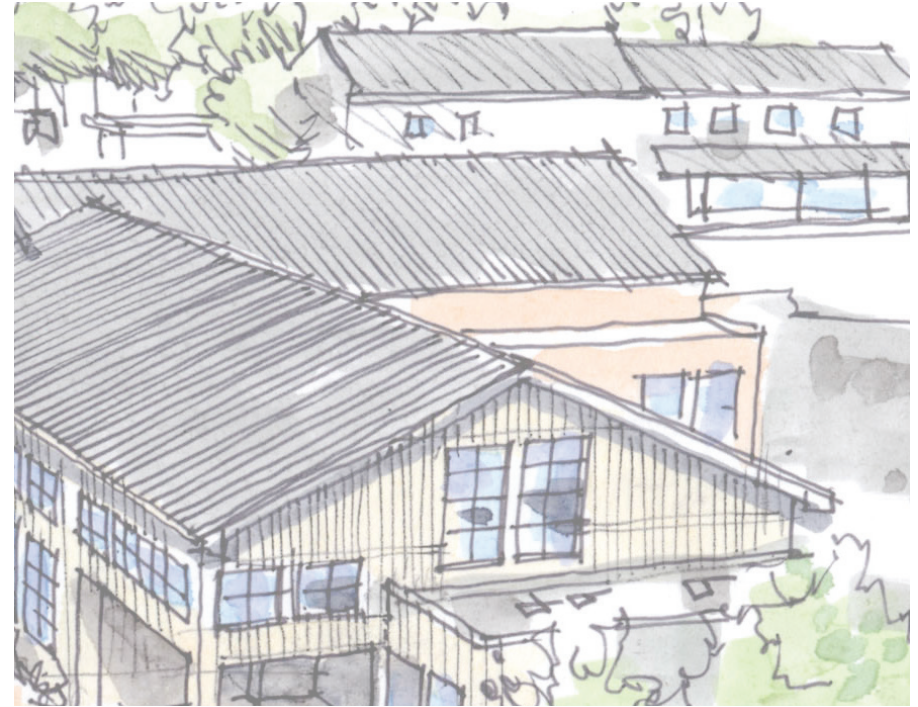
## FESTIVAL STREET

The proposed festival street will, in essence, be a linear pedestrian plaza that allows some vehicular passage. Cars will travel slowly through a narrow, designated drive aisle, which can be closed off to traffic when special events are desired to be held within the corridor. The entire space will flow seamlessly from drive aisle to edge of right-of-way to function as a pedestrian plaza and traffic-calmed woonerf, which will link to a pedestrian-only bridge across the Clear Creek to access the Argo site. Key principles for successful implementation of this special, flexible-use street type include creative use of visible entrances, physical barriers, shared and paved space, landscaping and street furniture.

### Standards for Festival Street:

- Create a flexible space, designed for pedestrian activity as the priority use.
- Provide a 20' vehicular drive aisle width – no vertical curbs, instead differentiate spatial separation through changes in pavement material and use of street trees, bollards, raised planters, etc.
- Incorporate horizontal shifts in the drive aisle to reinforce low speeds. Highlight pedestrian priority within crossing zones through use of colored/textured pavements.
- Provide an 8' min. clear walkway width throughout, with larger central gathering areas for outdoor dining, children's play areas, street performances, etc.
- Incorporate vertical focal points – water fountains, public art, shade elements, etc.
- Vary the adjacent building facade and setback depth to provide pedestrian interest.
- Provide a gateway feature – use design elements such as a raised speed table sidewalk crossing, overhead banners, parallel parking, raised planters, etc. to help define the entry.
- Include space for clustered bicycle parking areas.
- Include space for potential food truck vendors.
- Consider use of rain gardens, permeable pavers, and other low-impact stormwater treatments.
- Connect to Clear Creek Trail with stairs and/or ramp.







# 5

## Development Potential & Zoning

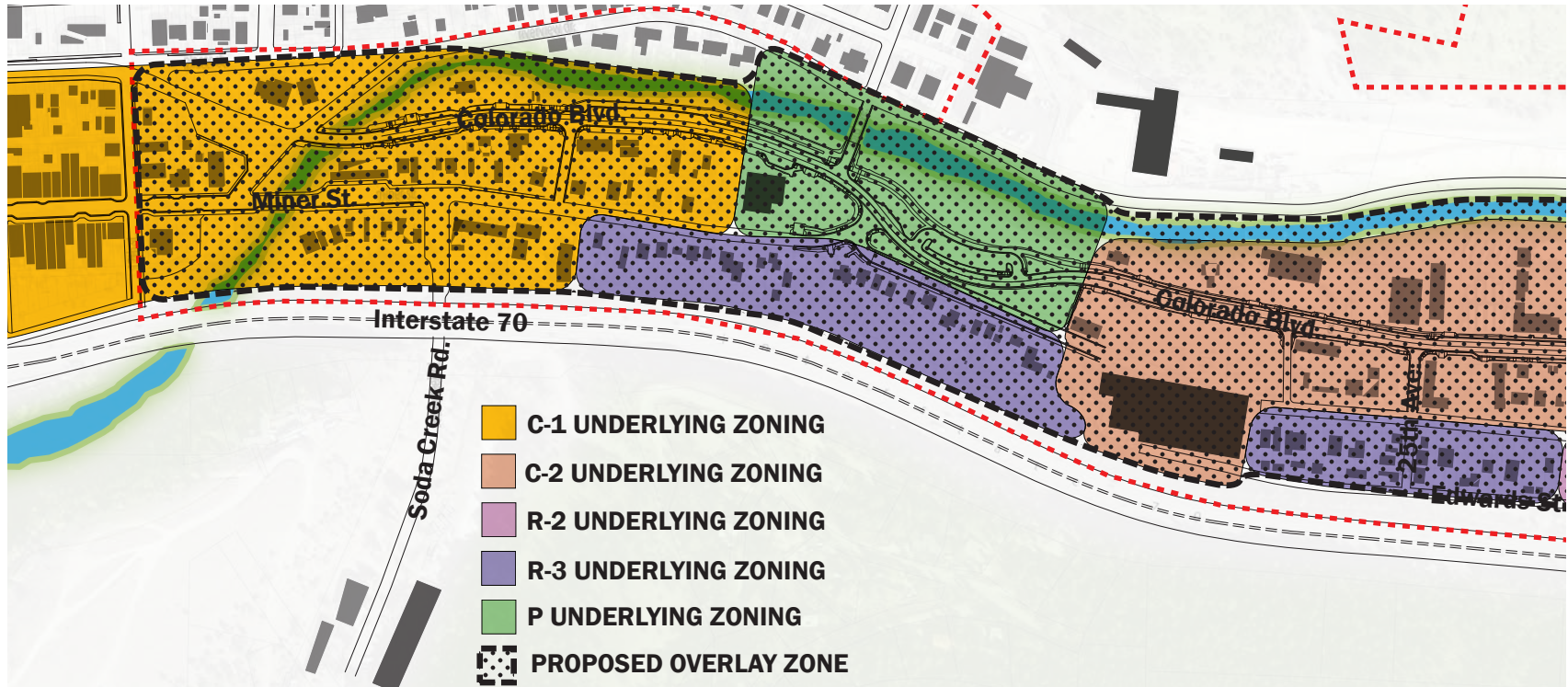


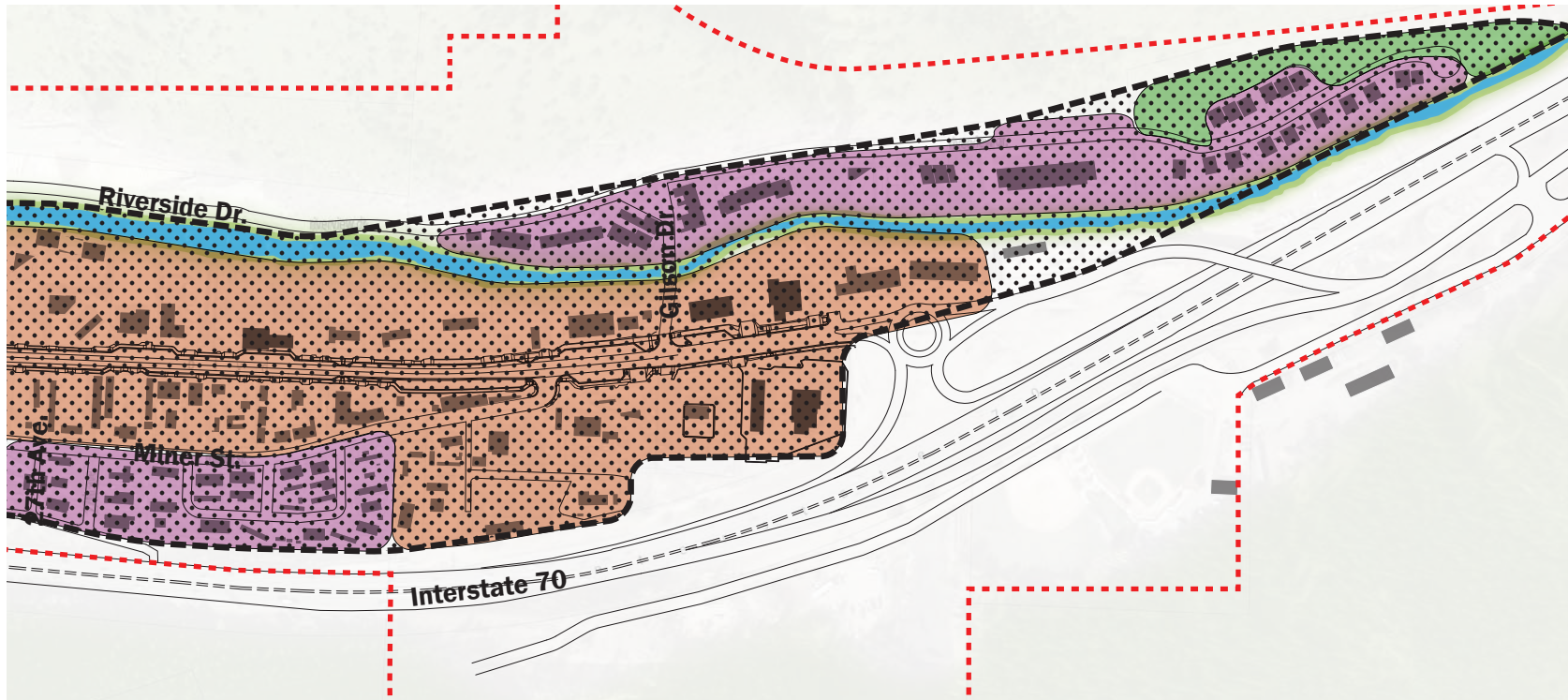
Figure 10 - Overlay Zone

## OVERLAY ZONE

The purpose of the proposed East End Overlay District is to support the implementation of the vision, goals, and policies outlined in the Idaho Springs East End Action Plan. More specifically, the zone districts are intended to:

- 1) Support continued revitalization of and reinvestment in the East End through targeted infill/redevelopment and adaptive reuse
- 2) Enhance connectivity and promote a pedestrian-oriented environment along Colorado Boulevard and Miner Street, and within the East End generally
- 3) Support a mix of locally-serving and tourism-related uses
- 4) Increase the range of housing options available in the East End
- 5) Promote innovative, high-quality, sustainable development





The East End Overlay District shall be applicable to all properties located within the East End Action Plan boundary that is zoned R-2 Residential Two District, C-1 Commercial One District, or C-2 Commercial Two District provided that the property meets one or more of the following criteria:

- 1) An infill project proposed for a vacant or substantially vacant lot within the East End Action Plan boundary
- 2) A redevelopment project proposed for a developed lot within the East End Action Plan boundary where all or most of the existing structure would be demolished and a new structure or structures built
- 3) An addition or renovation project proposed within the East End Action Plan boundary where the total square footage of the proposed addition is greater than twenty percent (20%) of the total square footage of the existing principal structure
- 4) An addition or renovation project proposed within the East End Action Plan boundary where the cost of the improvements is greater than thirty percent (30%) of the assessed value of the existing improvements (as shown in tax records)

The base zone districts within the East End Action Plan boundary shall continue to apply unless modified by these terms.

#### East End Overlay District:

*Please refer to the East End Overlay District in the Municipal Code (Chapter 21, Article III, Section 46) for more information.*



## DEVELOPMENT STRATEGIES

The speed and character at which development occurs in the East End is influenced by factors such as landowner interest, zoning regulations, and developer motivation. Development can occur organically to better respect and fit into the existing character, or in a highly planned matter which can allow for larger planning moves and infrastructure improvements. Given Idaho Springs fine texture and character, most development will likely occur in a slower, more organic manner unless adjacent land owners are interested in selling their properties for redevelopment.

Three primary development strategies are available to private property owners that support the overall vision:

- 1) Renovate existing property/minor improvements
- 2) Renovate existing property plus addition
- 3) Redevelop with new buildings and infrastructure

The planned streetscape improvements along Colorado Boulevard will improve pedestrian movement along the corridor and will provide a better framework for businesses to front the street.

(Above) Before and after adaptive reuse of Salvation Army store on West Colfax in Denver.

### Grant Assistance:

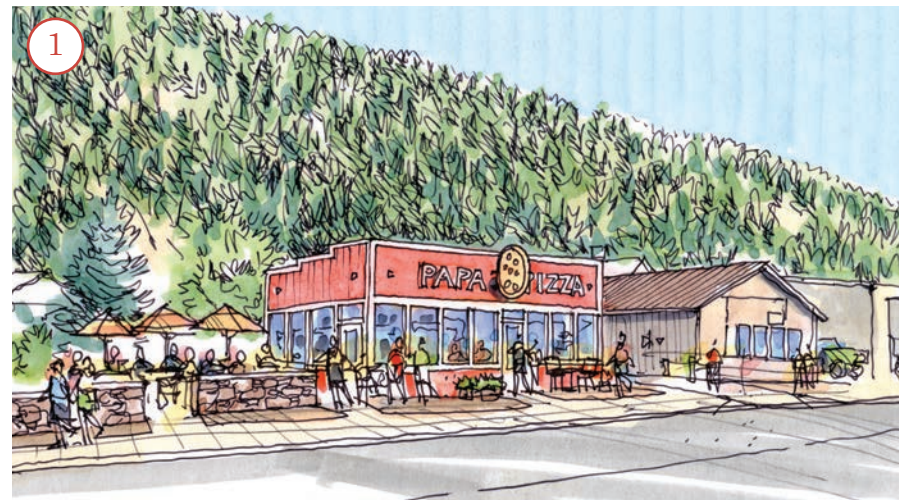
*Currently the City is exploring options to provide existing land owners with grant opportunities for site and building improvements. Interested property owners should contact the City for additional information.*





#### EXISTING CONDITIONS

Sample site area on Colorado Boulevard in its current state.



#### RENOVATE EXISTING PROPERTY/MINOR IMPROVEMENTS

The simplest way to improve the character of the East End is through property renovations and improvements. Partial grant opportunities exist for landowners interested in improving their property.



#### RENOVATE EXISTING PROPERTY PLUS ADDITION

In areas with low building coverage, building additions could be integrated adjacent to, or addition to, existing structures. This helps maintain the character of important structures while still integrating new development.



#### REDEVELOP WITH NEW BUILDINGS AND INFRASTRUCTURE

In areas where redevelopment could occur, existing businesses could have the opportunity to move into renovated spaces while providing opportunities for improved public improvements such as sidewalks and street trees.





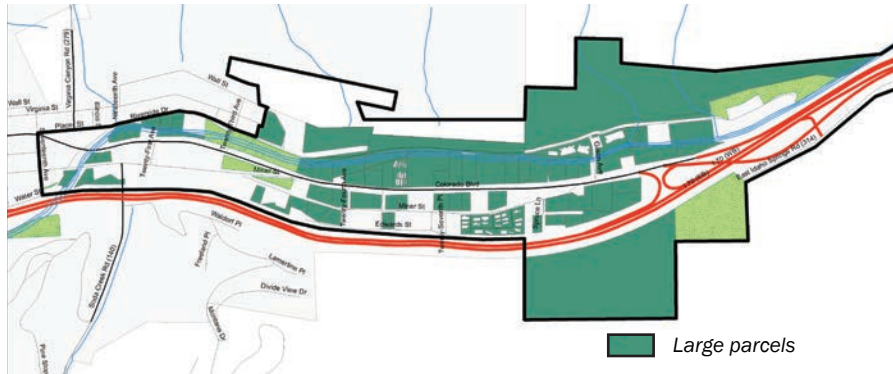
## METHODOLOGY

In an effort to determine potential sites for future development and reinvestment, a multi-tiered approach was utilized. This approach utilized GIS (geographic information systems) analysis and considered multiple factors such as:

- 1) **Large parcels**
- 2) **Low building to land value**
- 3) **Low F.A.R. (Floor area ratio)**
- 4) **Vacant or underutilized parcels**

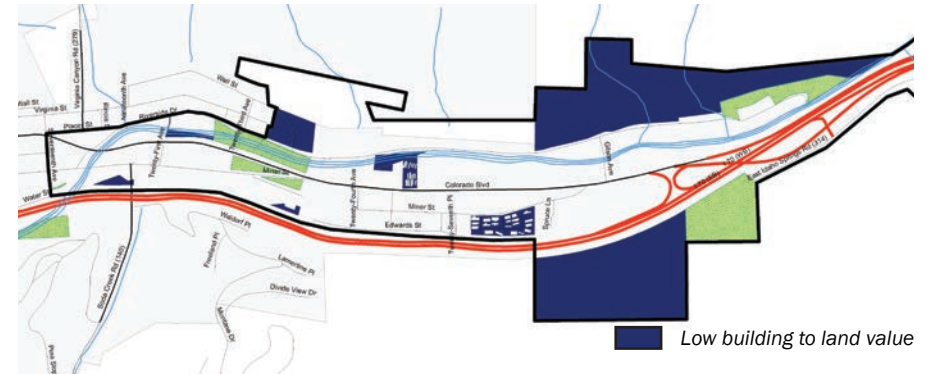
In addition to these factors, an in-field site analysis identified other constraints which may hinder development such as: vehicle noise, access, utilities, topography, and existing vegetation. Combining these factors, several test fit zones were selected to serve as a testing ground for the various programmatic uses that were identified in the public outreach sessions and through discussions with city representatives and economic consultants. These zones are conceptual and are not meant to be interpreted as a recommended development program for any one site.





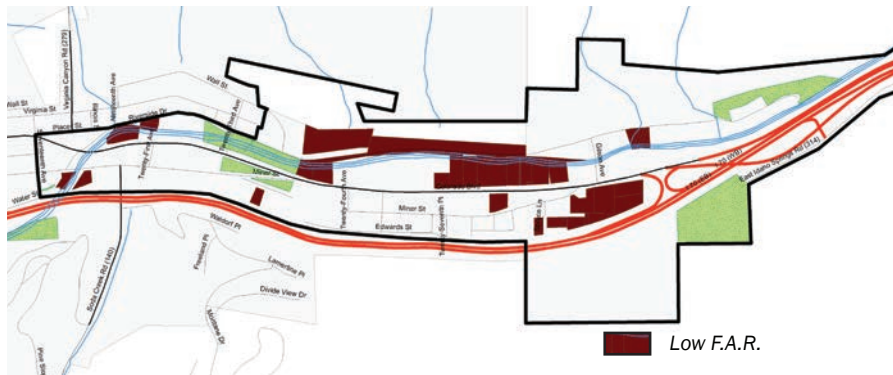
### Large Parcels

Parcels over ¼ acre in size were determined to be large enough to accommodate a redevelopment project. Most large parcels in the study area are on the north side of Colorado Boulevard; large parcels also include commercial parcels at the east end of Colorado Blvd by I-70, and parcels currently occupied by mobile homes.



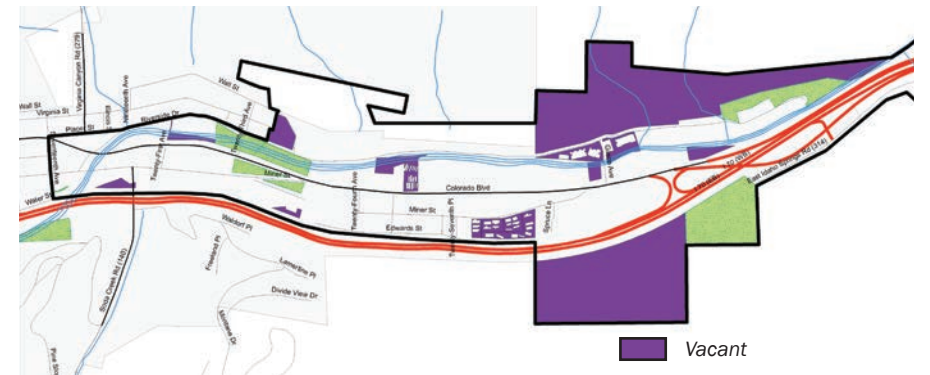
### Low Building to Land Value

Another factor used to identify underutilized parcels is the ratio of building value to land value, which provides an indication of development capacity. For this analysis, a threshold ratio of 0.5 was used to identify low-value parcels. Those with low value ratio include parcels currently being used as mobile home parks, as well as the vacant land next to the ball fields.



### Low F.A.R. (Floor Area Ratio)

A low Floor Area Ratio (FAR) indicates that a building may be underutilizing the land it is on and there is capacity for additional development uses or density. For this analysis, a threshold of 0.15 FAR was used to identify parcels that may be underutilized. Most parcels within the study area that meet this threshold are the commercial parcels at the east end of Colorado Boulevard by the I-70 interchange, and parcels on the north side of Colorado Boulevard.



### Vacant or Underutilized Parcels

While vacancy is often used as a factor in determining development potential, there are not many vacant parcels in the study area. The largest vacant site is to the west of the ballfields on the south side of I-70, however this site does not have municipal services and most of it is on a steep hillside, making development difficult. Some parcels that are identified as vacant in the county assessor database are currently occupied by mobile homes.



Figure 11 - Test Fit Sites

## TEST FIT SITES

As a result of the economic real estate analysis, two categories of test fit sites were created for short-term and long-term development potential:

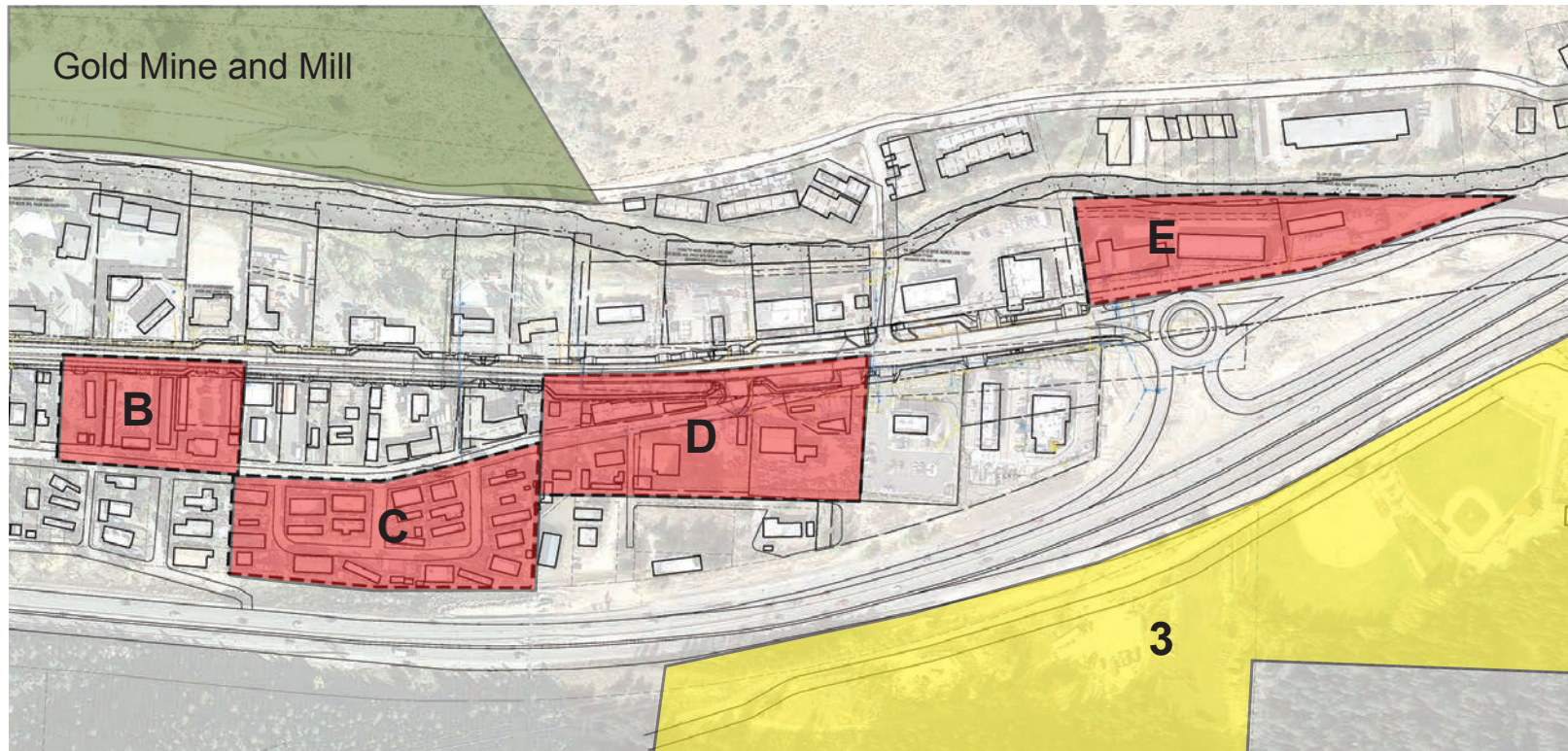
### 1) Program Level Sites (A, B, C, D, and E)

### 2) Vision Level Sites (1, 2, and 3)

These sites are not intended to be actual development proposals, but rather “test fits” to certain sites where our analysis showed potential opportunity. Based upon the community needs assessment which gathered input from community members, city staff, and an economic specialist, several key land use and development opportunities rose to the top. Listed below are the market-driven programs and approximate 5-10 year threshold:

- **For-sale housing (market rate townhomes and workforce single family)**  
25-50 units
- **Affordable rental apartments** - 75-100 units
- **Highway-oriented commercial** - 10,000 - 15,000 square feet
- **Lodging (limited service chain or boutique)** - 60-80 rooms
- **Mixed Use development** - 5,000 square feet local retail





#### PROGRAM-LEVEL SITES

Through this analysis, five sites (A-E) were selected for further study in terms of potential architectural programming and massing. These are seen as catalyst development opportunities that could accommodate the forecasted residential and commercial development potential and help the East End achieve its overall vision.



#### VISION-LEVEL SITES

These three sites (1-3) also have potentially good long-term value, but are not studied at the detail of the others. Instead, these sites are programmed in general terms, indicating potential highest and best uses for their location. These projects would contribute to the vitality and vision for the East End, but may take longer to develop due to known constraints.





## TEST SITE A

Study Size Area  
1.3 ACR  
(Gross)

Current Land Use  
Idaho Springs Lumber and Hardware  
(vacant)

Current Zoning  
C-1

Attraction  
Vacant lumber yard on large parcel  
with proximity to hot springs and  
downtown



(Above) Early concept sketch showing park and health clinic option

Located in the western portion of the study area, Test Site A is at the key intersection of Miner Street and Soda Creek Road where the now vacant Idaho Springs Lumber and Hardware is located. This intersection receives substantial vehicular travel as Miner Street serves as the primary east-west thoroughfare connecting downtown Idaho Springs to the East End and Soda Creek Road providing a key southern connection to Indian Hot Springs and mountain residences.

The site consists of approximately 1.3 acres (gross) and features a daylit segment of Soda Creek. Three options were studied with different site programs which include:

- 1) Health Clinic with Small Park
- 2) Single Family Residences
- 3) Multi-family Residences

Through the community review process, Option A-2 received the best feedback due to its scale and connection with the adjacent development.





**OPTION A-1**  
Health Clinic with Small Park



**Option A-1 Program:**

- 8,000 SF clinic
- 8,000 SF (0.18 acre) pocket park
- Soda Creek daylit through site (amenity area and stormwater)
- 52 surface parking spaces



**OPTION A-2**  
Single Family Residences



**Option A-2 Program:**

- 10 single family homes
- Soda Creek daylit through site (amenity area and stormwater)
- Parking located in private garages



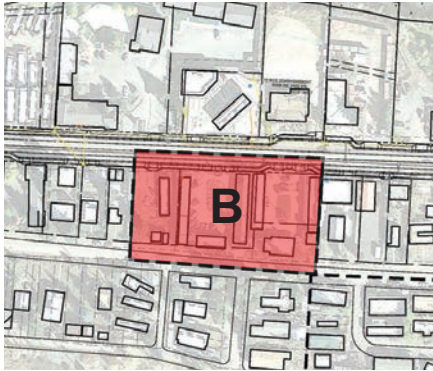
**OPTION A-3**  
Multi-family Residences



**Option A-3 Program:**

- 18 dwelling units (rental)
- Soda Creek daylit through site (amenity area and stormwater)
- 24 tuck-under parking spaces
- 11 surface parking spaces





## TEST SITE B

Study Size Area  
1.47 ACR  
(Gross)

Current Land Use  
*Motel*

Current Zoning  
C-2

Attraction  
*Central location with Colorado  
Boulevard frontage*



(Above) Preliminary sketch of Option B-1I development scenario.

Centrally located along Colorado Boulevard in the East End study area, Test Site B consists of several larger parcels which currently includes two older motels. These parcels, among other potential parcels, could be seen as an opportunity by developers for a mixed-use multi-family building. This use would promote pedestrian activity along Colorado Boulevard through an active retail ground-floor and could feature uses such as:

- 1) Restaurants
- 2) Boutique Retail

Additionally, a north-south connection between Colorado Boulevard and Miner Street would help promote slower vehicular traffic while providing active modes of transportation such as walking and cycling.

A key consideration in this area is how potential development would transition and blend into the fabric of the single family homes along Miner Street. This could be accomplished through maximum height limits and the stepping of building masses to contextually fit with adjacent development.





**OPTION B-1**  
Restaurants

**Option B-1 Program:**

- 11,500 SF retail/restaurant space
- 16 dwelling units (rental)
- 53 surface parking spaces
- 15 on-street parking spaces



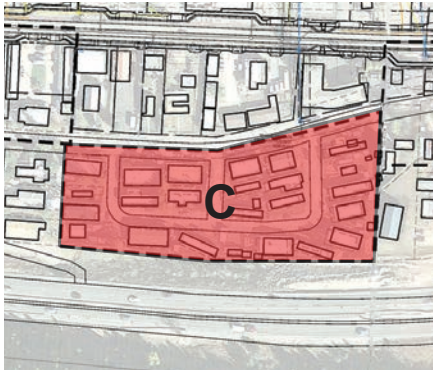
**OPTION B-2**  
Boutique Retail

**Option B-2 Program:**

- 9,500 SF retail/restaurant space
- 40 dwelling units (rental or condo)
- 16 surface parking spaces
- 60 below-grade podium parking spaces
- 15 on-street parking spaces







## TEST SITE C

Study Size Area  
2.55 ACR  
(Gross)

Current Land Use  
*Manufactured housing*

Current Zoning  
R-2

Attraction  
*Large parcel with single owner,  
low building to land value, and  
low density.*



(Above) Preliminary sketch along Miner Street showing small cottage development similar to the existing development.

Tucked adjacent to, but below, much of the noise of I-70, Test Site C currently consists of a 2.55 acre (gross) manufactured housing community with approximately 20 units. As the largest parcel identified in the test fit study, this zone offers a high potential for redevelopment.

Various development scales were studied but with the existing cottages along Miner Street, the continuation of this building type seemed to provide the most appropriate character and density. This area could offer market-rate and affordable housing alternatives to create a mixed-income housing community. If desired, additional density and housing diversity could be created through the integration of duplex units. Taller architecture would block views into the town, create units with undesirable views, and create inconsistent character in the neighborhood.

Feedback received in community meetings was positive for this development scenario due to its scale, character, and connection with the adjacent single family neighborhood.



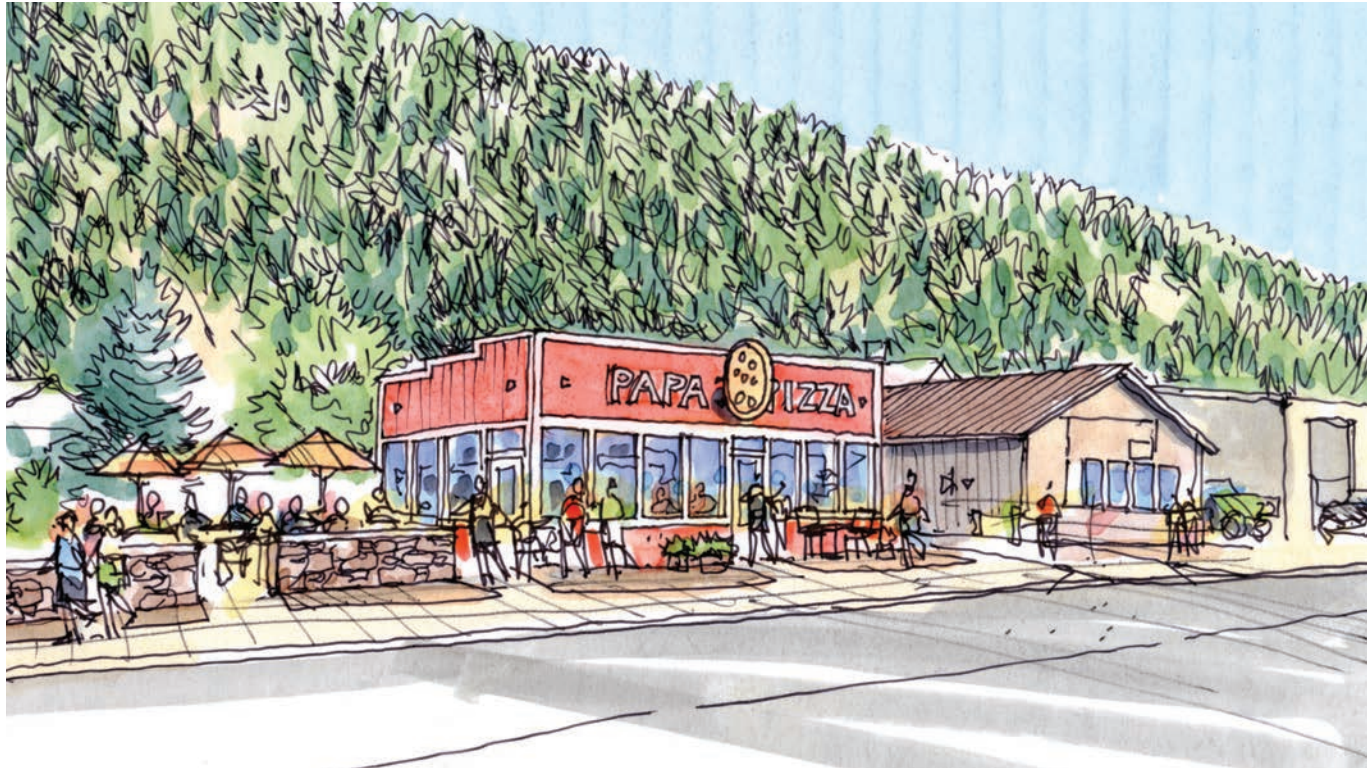
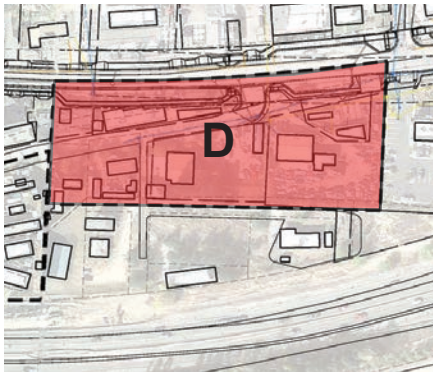


**Option C-1 Program:**

- 23 single family homes
- 2 open space courtyards (shared amenity space)
- Parking located in private garages







## TEST SITE D

Study Size Area  
3.0 ACR  
(Gross)

Current Land Use  
*Commercial, single-family  
residential*

Current Zoning  
C-2, R-2

Attraction  
Low F.A.R.  
(Floor to Area Ratio)

Seeking a location for additional housing and commercial opportunities, Test Site D was chosen due to its low F.A.R. (floor to area ratio), adjacency to Interstate 70, and its location along the soon-to-be improved Colorado Boulevard.

Test fit studies were completed at various densities to visualize different levels of housing and commercial mixes. Development that fronts Colorado Boulevard and promotes pedestrian movement will encourage visitors to continue west to discover more of Idaho Springs. Meaningful residential and commercial buildings exist within this zone but, due to their fragmented placement, they do not create a strong sense of place and do not promote walkability. If working together, business owners and the city could create a more vibrant node that would attract more customers to this area. This requires a collaborative and interactive process between the city and businesses.

These studies could develop along other portions of the Colorado Boulevard corridor and the final result would be organic blend of historic and new buildings. This blend of old and new would have the ability to create a more interesting mosaic of architecture and would naturally build in the affordability of historic buildings into the fabric of the neighborhood.

*(Above) Early concept sketch showing several existing buildings to remain with site and building improvements.*





OPTION D-1



**Option D-1 Program:**

- 10,500 SF retail/restaurant space
- 32 dwelling units (rental)
- 93 surface parking spaces
- 37 on-street parking spaces



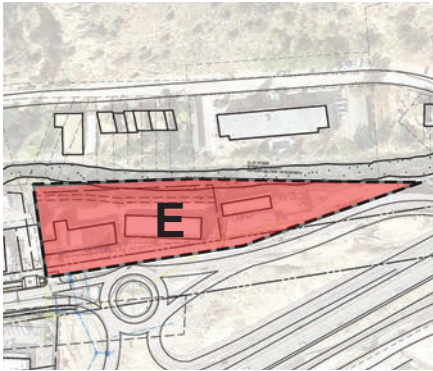
OPTION D-2



**Option D-2 Program:**

- 6,000 SF retail/restaurant space (new buildings)
- 3,000 SF retail/restaurant space (adaptive reuse)
- 32 dwelling units (rental)
- 89 surface parking spaces
- 37 on-street parking spaces





## TEST SITE E

Study Size Area  
2.1 ACR  
(Gross)

Current Land Use  
*Motel, police headquarters*

Current Zoning  
C-2

Attraction  
*Entry gateway with creek  
adjacency*



(Above) Preliminary sketch of option E-2 showing limited services hotel adjacent to the new roundabout.

As the eastern gateway to Idaho Springs, Test Site E provides the first glimpse of Idaho Springs to visitors from the east. This highly-trafficked area sees a lot of customers to the interstate commercial businesses for fuel, fast-food, snacks, and other convenience-based needs. These businesses attract tourist dollars and have the ability to generate substantial tax revenue for the city. McDonalds currently provides the largest sales tax revenue in the city.

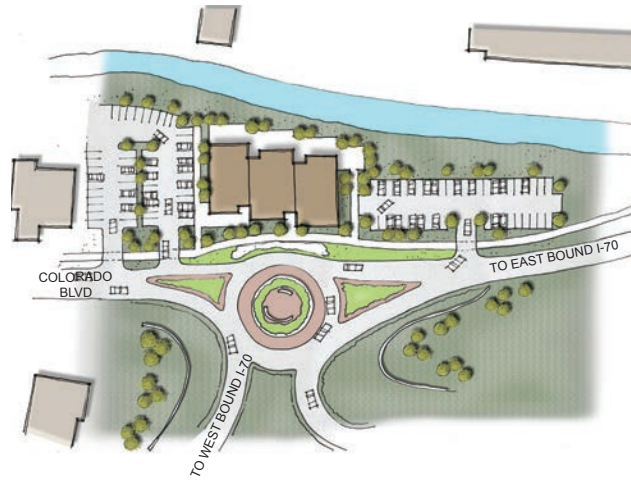
Several alternatives were studied including options for the following:

- 1) Restaurants or Retail
- 2) Hotel

Both studies have the opportunity for exterior dining or gathering spaces adjacent to the creek and would benefit from an enhanced entry gateway and monument. Although the newly constructed roundabout is an improvement of the past conditions, much can still be done to improve the entry sequence to and from Exit 241. Improvements to signage, landscaping, and lighting could substantially improve the image visitors have on the City.

Feedback received in public meetings expressed concern over the height and feasibility of a hotel in this area.





**OPTION E-1**  
Restuarants or Retail



**Option E-1 Program:**

- 15,000 SF retail/restaurant space
- 65 surface parking spaces
- Improved roundabout and "gateway" beautification



**OPTION E-2**  
Hotel



**Option E-2 Program:**

- 70-room hotel (chain)
- 70 surface parking spaces
- Improved roundabout and "gateway" beautification







## VISION LEVEL SITE 1

Study Size Area  
0.5 ACR  
(Gross)

Current Land Use  
Office

Current Zoning  
C-1

Attraction  
Adjacent to creek and  
downtown

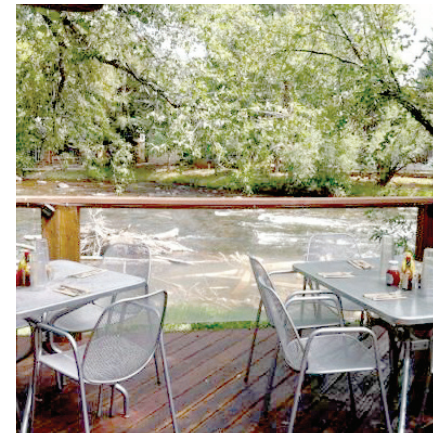
Situated at a unique location adjacent to Clear Creek and downtown Idaho Springs, Vision Level Site 1 has several uses which could contribute to the character of the surrounding community. These uses may include the following:

- 1) Restaurant with large outdoor dining area
- 2) Maker space
- 3) Coworking space
- 4) Boutique retail

Independent of these uses, there exists an opportunity to engage and view the river. This could be established in different ways depending on the use but would ideally include a more accessible slope that allows people to gather closer to the creek edge. Careful consideration for the adjacent floodplane must be considered when developing along the creek edge.

(Above) Preliminary sketch of potential development scenario.



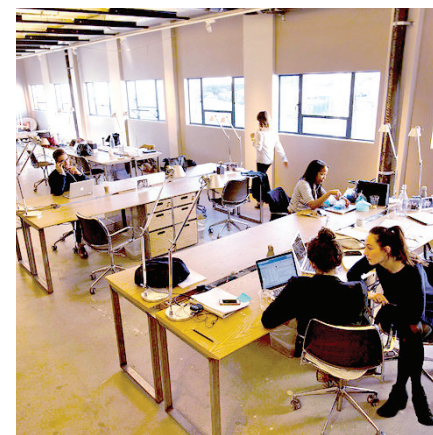
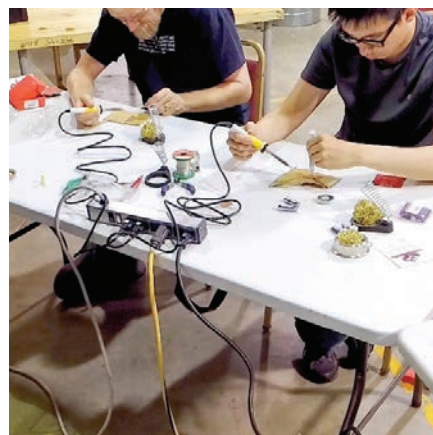
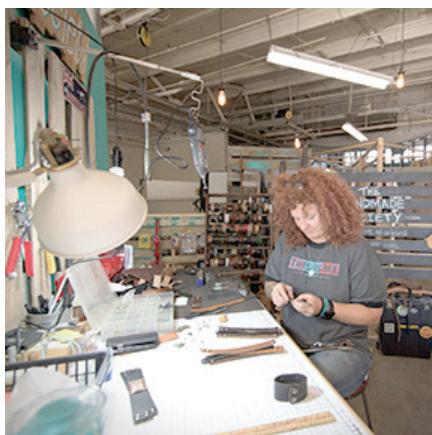


### Restaurant with Dining Terrace

*With premium creekside frontage, this zone offers a high potential for outdoor dining which is an amenity many of the downtown restaurants currently lack due to a minimal sidewalk area along Miner Street. This zone sits below I-70 and the white noise of the creek should help damper some of the traffic noise.*

*With travelers along I-70, rafters on Clear Creek, and frontage along Miner Street, this is a high visibility site located adjacent to the well established downtown.*

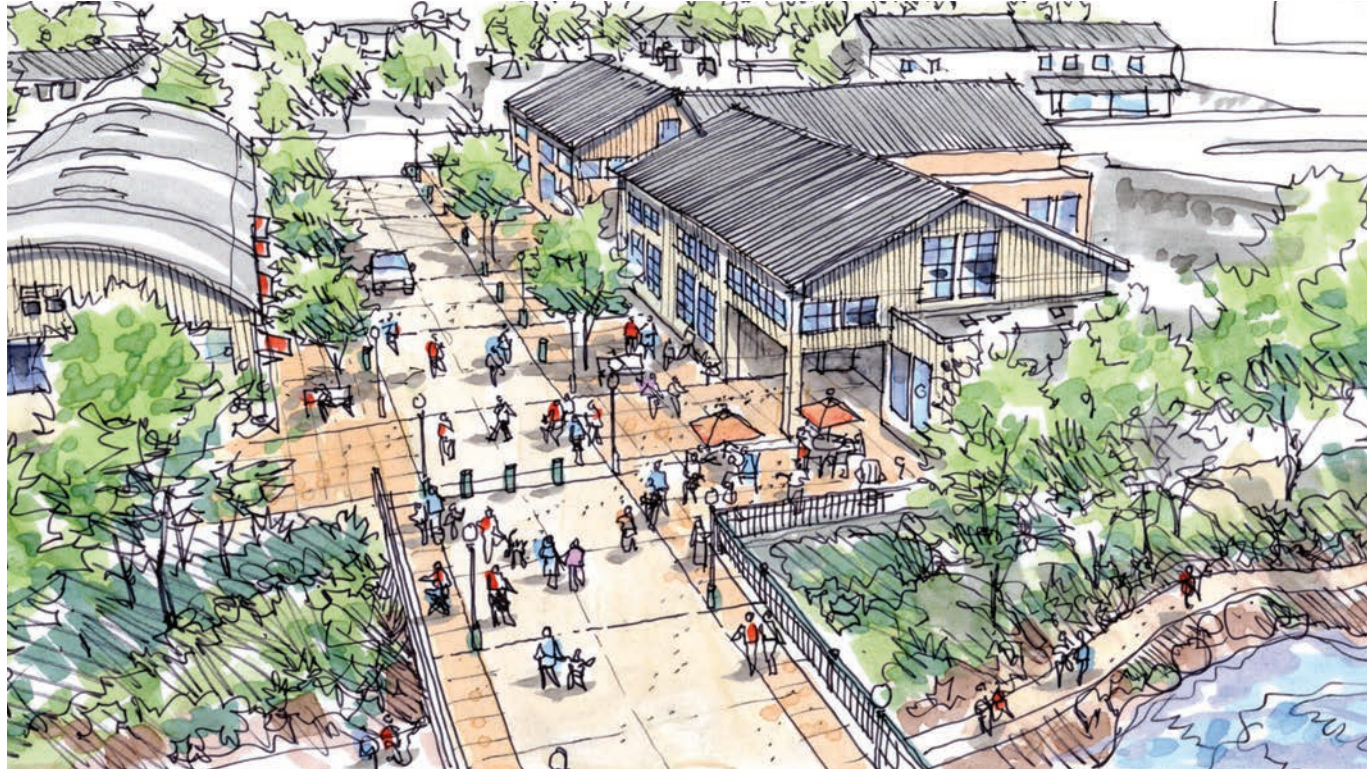
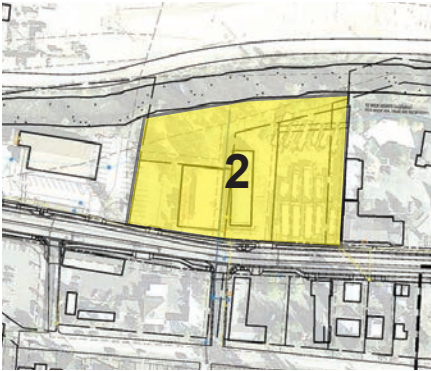
*When making improvements the floodplain must be considered to ensure the proper frontages are established.*



### Maker Space/Coworking Space

*Although not contributing to the architectural character of the downtown, the existing two story building could be utilized for a maker or coworking space. Maker spaces are rented work spaces for artisans that have communal access to tools and resources that would be hard to own or maintain independently. The cities industrious history could lend itself well to this use. In an effort to attract young visionaries to the area, coworking spaces provide rentable work spaces for small businesses or remote employees in a communal atmosphere. These spaces are typically popular among technology based employees and feature business speed internet and flexible workspace options.*





## VISION LEVEL SITE 2

Study Size Area  
2.4 ACR  
(Gross)

Current Land Use  
Automotive store, music school,  
manufactured homes

Current Zoning  
C-2

Attraction  
Adjacent to Argo and creek

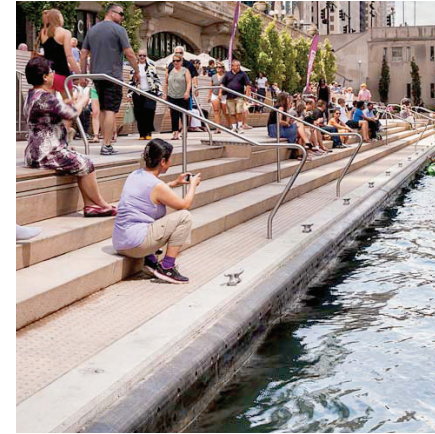
Across the Creek from the iconic Argo Mill, Vision Level Site 2 includes an existing automotive store, music school, and a small community of manufactured homes. Current plans are in the works to develop affordable housing where the manufactured homes currently exist.

Two bridges currently exist in the study area which are at 23rd Avenue and Gilpin Avenue (weight restricted). These bridges are 2/3 mile from each other which discourages pedestrian movement. Providing an additional pedestrian bridge connection could help provide access to the Argo property and the other side of the creek. A pedestrian plaza and festival street is envisioned for this area due to its alignment with the base of the Argo and it is a midpoint between the existing bridges. This connection is flexible and additional discussion can be found in the Chapter 4 Connectivity chapter.

The development of a festival street would create a walkable pedestrian friendly zone that would mimic downtown Idaho Springs and would include retail establishments such as restaurants and boutique stores. This area would feature transparent and permeable groundfloor design which would help enliven the streetscape experience.

(Above) Preliminary sketch of festival street connecting from Colorado Blvd to the Argo Mill.





### Creek Connection

East of CRC Park, there are minimal opportunities to view and engage the water due to the steep slopes and large rip-rap. In select high-impact areas, finding opportunities to create a harder edge with a more accessible slope would capitalize on the creek amenity that is currently underutilized.

Respecting the creek's natural ecology, high discharge rates (CFS), floodplane, and peak heights are key considerations when designing these areas. The proposed design must accommodate the natural fluctuations in the creek corridor and make safety a high priority. Additionally, any new bridges must provide the proper clearance for rafting along the creek.



### Festival Street

Hosting special events that gather regional interest on the East End require the proper space and outside of CRC Park, minimal opportunities exist that would not cause substantial gridlock during peak visitation.

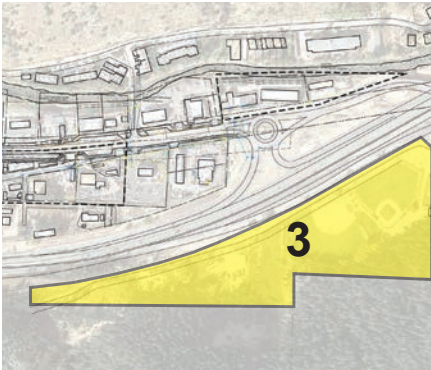
A festival street would be located off of Colorado Boulevard to ease congestion and would become a pedestrian only zone during special events. The buildings that frame the festival street would be retail establishments such as restaurants and boutique stores similar to those that currently exist in downtown Idaho Springs.

Programming is key to success and this space would be an additional venue that could work with in unison with the future renovation of CRC Park. Potential activities may include the following:

- Summer concerts
- Outdoor movies
- Mining heritage festival
- Farmers market
- Taste of Idaho Springs







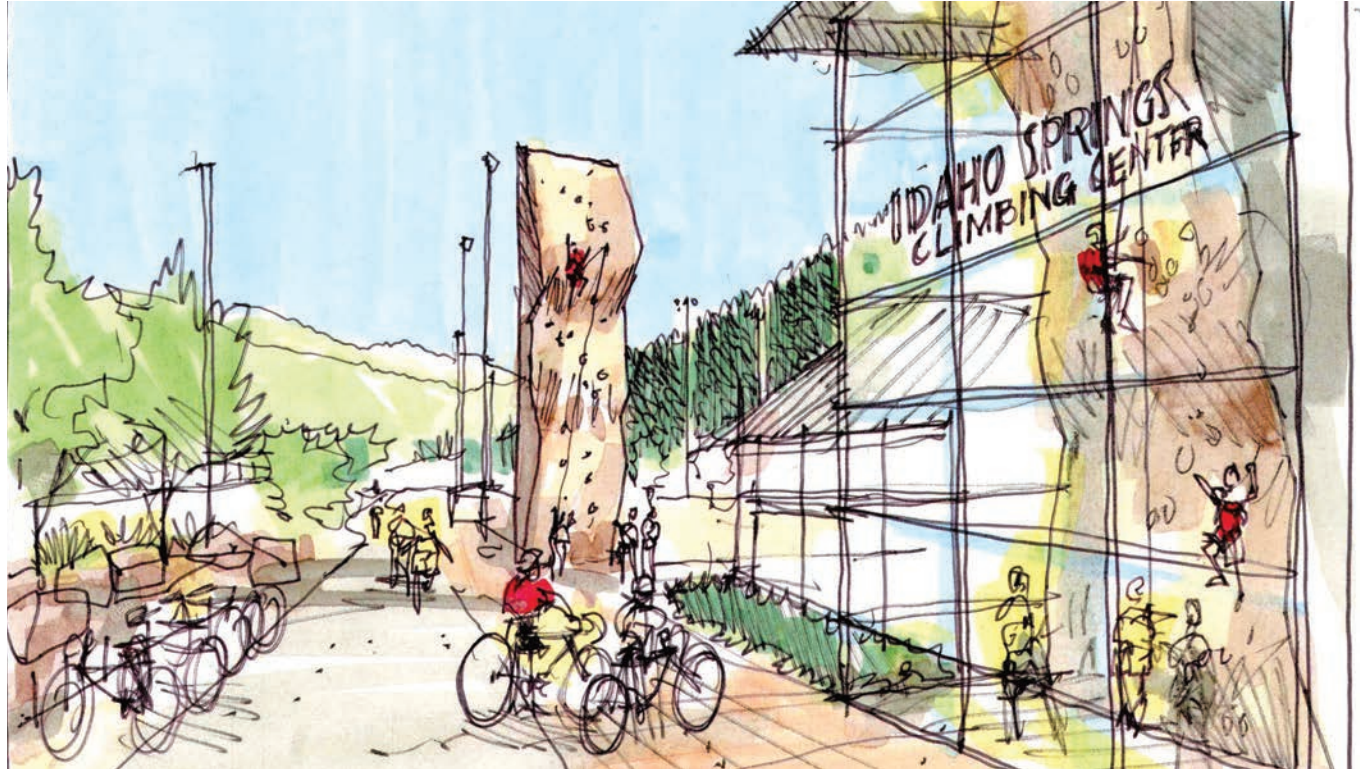
## VISION LEVEL SITE 3

Study Size Area  
10.4 ACR  
(Gross)

Current Land Use  
Recreation fields, vacant

Current Zoning  
PUD

Attraction  
Large parcels and partially  
owned by the public



(Above) Early sketch of climbing gym concept along the multi-use trail.

Abutting the southern edge of I-70, Vision Level Site 3 consists of publicly and privately held land. The publicly owned land features the following site programming features:

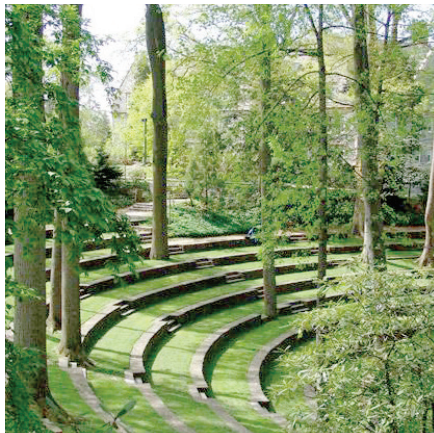
- 1) Baseball field with multi-sport striping
- 2) Softball field with multi-sport striping
- 3) Bandshell

The privately owned land has several level tiered gravel pads clear of vegetation and could be best used for the following uses:

- 1) Park expansion
- 2) Recreation facility
- 3) Multi-family residential (limited by hillside)

Challenges facing development include regional overhead electrical lines, road noise, topography, mature vegetation, low sunlight, utility connections, and its disconnected location. These challenges would likely require a substantial offset or buffer from I-70 if residential uses were developed. Positive features of this zone include its visibility from Interstate 70, woodland backdrop, and frontage on the regional multi-use path that goes through Idaho Springs.

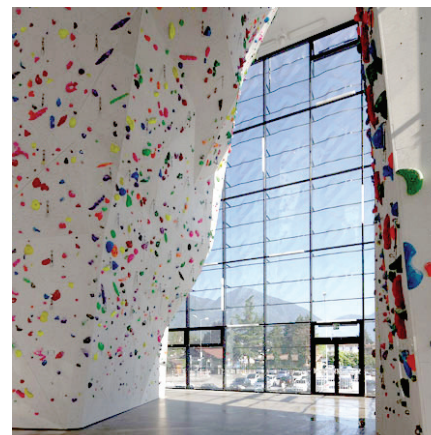




## Park Expansion

An expansion of the adjacent recreational amenities could provide additional youth programming opportunities in concert with the Clear Creek Recreational Center. Several programming opportunities include:

- Mountain bike park
- Outdoor skating rink
- Dedicated bandshell
- Bike rental hub with trailhead for future trail expansion



## Recreation Facility

In concert with a potential park expansion, a public or privately held recreational facility could build upon the city's reputation for outdoor adventure sports. An indoor/outdoor climbing gym could be a popular use in this area. Given the close proximity to I-70, architectural features could act as a natural billboard for passing visitors and is a great opportunity for Idaho Springs to improve the image of the city.



## Residential Living

A modest amount of multi-family or single family housing could be integrated into the hillside. Smaller, flexible architectural footprints for single family residences would likely be the easiest and most feasible solution that would allow much of the existing terrain and tree canopy to remain. Without municipal services, additional infrastructure costs would be required for larger scale development. Residential development along Montane Drive east of the Indian Hot Springs probably faced similar challenges when developed previously.





## THE MIGHTY ARGO

Study Size Area  
+/- 25 ACR

Current Land Use  
*Historical landmark*

Current Zoning  
C-1, PUD

Attraction  
*Historic and cultural icon*

Undoubtedly the most iconic and meaningful structure in Idaho Springs, the historic Argo Mill is currently in the preliminary concept stages of an ambitious renovation and development plan. The future vision enhances the visitor experience and could include new amenities such as:

- 1) Hotel and conference center
- 2) Housing
- 3) Restaurant
- 4) Creekside bike path

The site is on the National Register of Historic Places and is an EPA Superfund site that requires careful environmental and site planning. Important considerations related to the East End Action Plan include the following:

- 1) Entry gateway
- 2) Site access and emergency egress







## Hotel and Conference Center

Current hotel trends suggest occupants are seeking smaller boutique experiences and the unique mining heritage of the Argo Mill and Tunnel have the ability to convey a compelling story that distinguishes itself from other lodging experiences in the region and nation.

Many of these hotels offer small art or exhibit collections which create a unique user experience and encourages other individuals to visit the space to view the exhibits. This hotel could draw visitor from local and national locales seeking a mountain adventure within close proximity to the Denver metro. It is important the styling is authentic to Idaho Springs and aligns with what visitors come to expect.



## Housing

A few small areas along the creek could lend themselves to modest residential development. This development would likely require some construction along the toe of the steep slope.

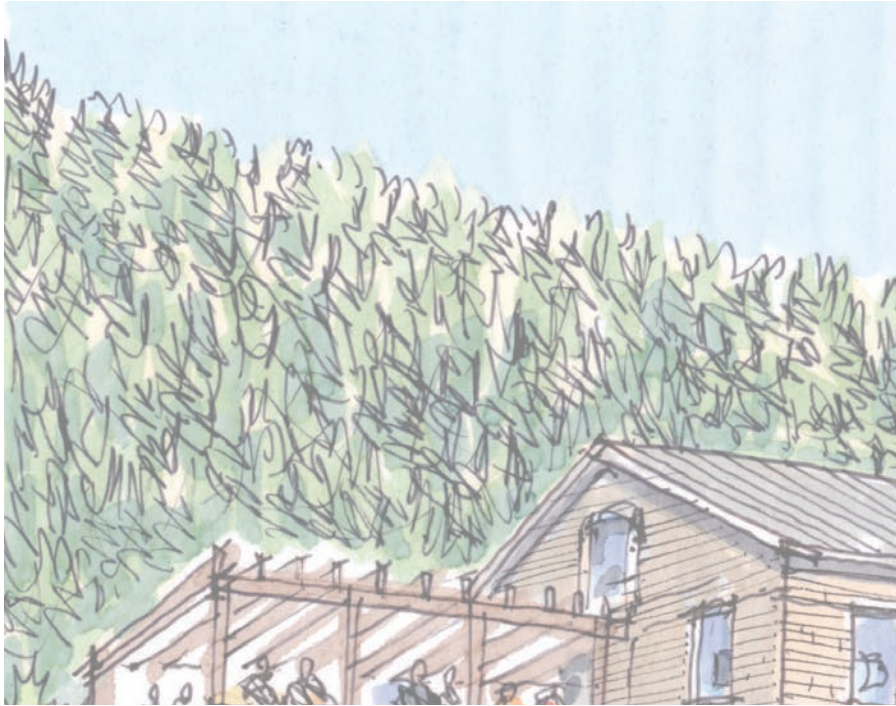


## Creekside Path and Retail

Small retail shops and restaurant could anchor the base of the Argo Museum and create a unique opportunity for businesses to front Clear Creek.

A creekside trail envisioned along the north side of the creek would be part of a regional trail that has the potential to attract a substantial amount of cyclists along the I-70 corridor. This regional connection allows local visitors the option to leave the car at home and reduces the burden on local roads and parking demand.







6

Implementation Strategy

## INTRODUCTION

The East End Action Plan is intended to represent the community's vision and goals for revitalization, redevelopment, business expansion, and community and recreation improvements over the next 5 to 10 years, with specific interest in generating investment in a short 1 to 3 year time frame. The Plan is intended to build upon the City's agreement with CDOT to take control of Colorado Boulevard and the related investment of over \$20 million in roadway improvements in the East End. The reconstruction of Colorado Boulevard will enhance the corridor for both pedestrians and vehicles, and provide opportunities for new and expanding businesses. The Plan establishes project goals and design principles and guidelines; provides an urban design framework addressing districts, corridors, parks and open spaces, and development uses; identifies the market opportunities for new development and reinvestment; and provides examples of the type of buildings and places that are consistent with community values.

This Implementation Strategy provides recommendations for specific programs, investments, and policy changes needed to implement the goals of the Plan. It is organized around six priority areas as a way to effectively leverage available resources and address important issues facing the community:

**Priority Area 1: Community Enhancements and Placemaking**

**Priority Area 2: Recreation and Tourism**

**Priority Area 3: Parking Management**

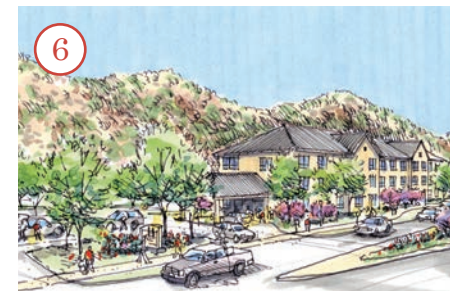
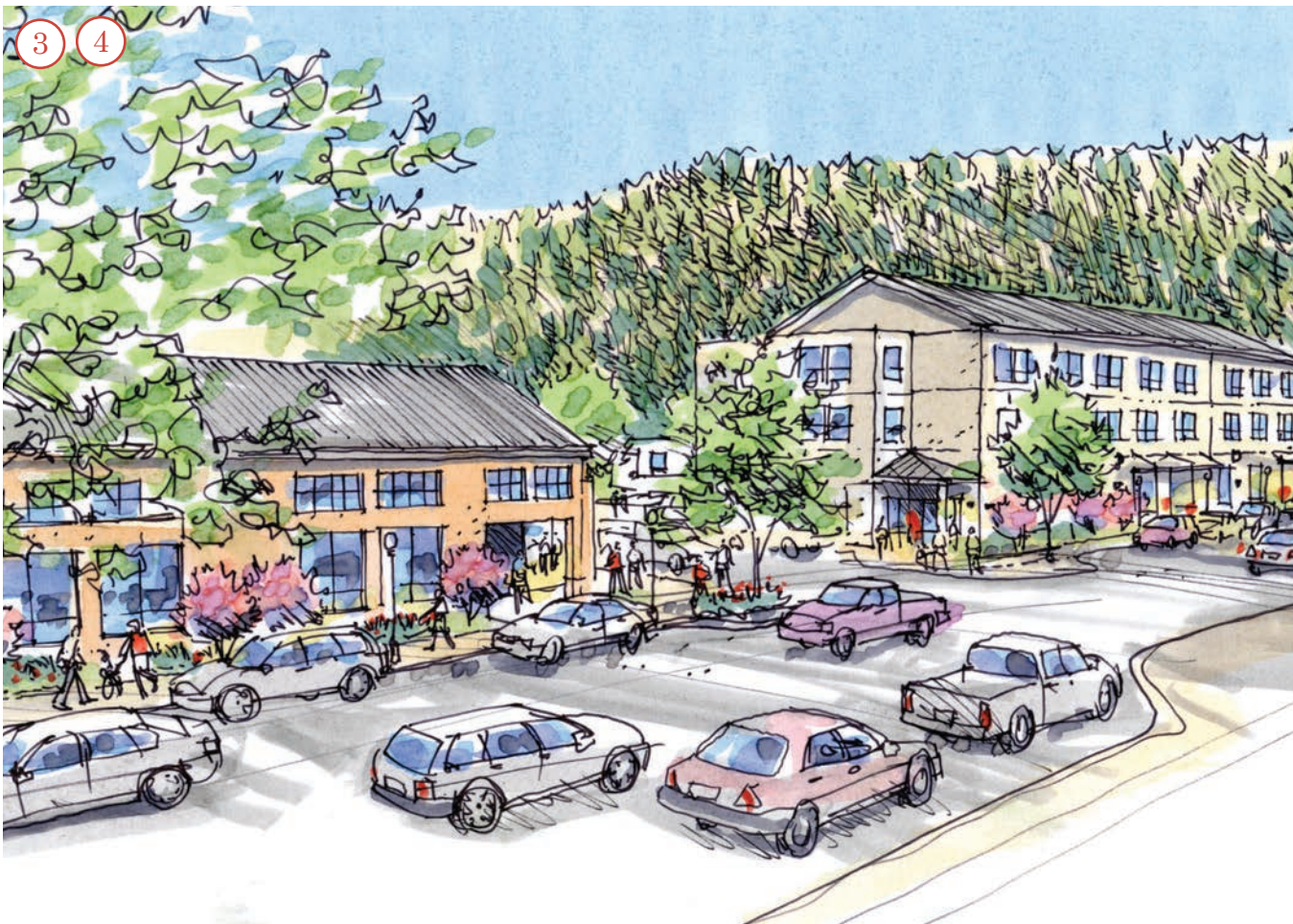
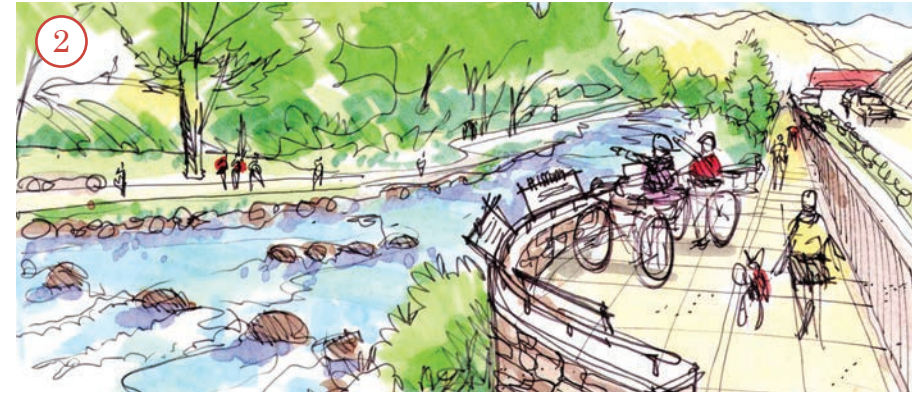
**Priority Area 4: Revitalization and Economic Development**

**Priority Area 5: Housing Development**

**Priority Area 6: Business Development**

These priority areas reflect overarching themes that emerged from the East End plan process and help frame a series of priority actions that the City and community can take to help achieve key goals and objectives in area as a whole and in the defined districts.









## PRIORITY AREA 1: COMMUNITY ENHANCEMENTS AND PLACEMAKING

### WHY IT IS IMPORTANT

Through a series of community events, open houses, and stakeholder interviews, corridor beautification and placemaking were key themes and priorities raised by the community. People expressed interest in providing access to Clear Creek and creating a vibrant, pedestrian-friendly district as an extension of downtown. Placemaking and urban design are key to making these goals come to life. Furthermore, creating highly desirable public spaces, a grid of smaller blocks, and “complete streets” that include accommodations for bicycling and walking positively affects the other community goals related to increased recreation/tourism opportunities and increased housing choices and economic development.

### OBJECTIVES

- 1) Strengthen the Identity of the East End as a whole, as well as within the individual character areas
- 2) Provide increased access to Clear Creek
- 3) Improve connectivity and walkability
- 4) Create a series of public and semi-public open spaces that offer areas for recreation and leisure activities
- 5) Create an “authentic place” that reflects the mining heritage and mountain aesthetics of Idaho Springs

### IMPLEMENTATION APPROACH

This community enhancement strategy seeks to increase community amenities, street connectivity and walkability throughout the East End to create a more connected and cohesive place. These objectives demand cooperation from both the public sector (city and county) and the private sector (property owners and developers).





## PRIORITY AREA 1 ACTIONS

	Action	Responsibility	Funding Source	Timeframe
P1.1	<b>Gateway and Enhanced Streetscaping Beautification</b> - Hire consultant to further design and documentation of beautification efforts. Begin with “gateway” landscaping enhancements. Other beautification efforts include enhanced streetscaping elements along Colorado Boulevard (benches, lighting, landscaping).	City	General Fund; Grants	2017 (Design) 2018 (Construct)
P1.2	<b>CRC Park Improvements</b> - Implement expanded CRC Park design and beautification and playground rehabilitation.	City	General Fund; RAMP Fund	2018
P1.3	<b>Signage and Wayfinding Master Plan</b> - Hire consultant to design a signage and wayfinding plan for Idaho Springs to create a better experience and direct people to destinations and parking efficiently.	City	General Fund; Grants	2018 (Design) 2019 (Construct)
P1.4	<b>Semi-Public Parks/Plazas</b> - Work with property owners and developers to implement other proposed parks/plazas, as identified in Plan.	URA; Property Owner/ Developer	TIF; Private Development	Ongoing
P1.5	<b>New Street Connectivity</b> - Work with property owners to implement desired connectivity by way of proposed new streets with “complete streets” elements, as identified in the Plan. <b>Phase 1</b> should include connections to the Argo Mill, including a new street and bridge over Clear Creek (approx. 575-feet west of Gilson) and flexible “festival street” (approximately in alignment with 25th Ave. from Colorado Blvd. to Clear Creek) with pedestrian plaza/bridge over creek. <b>Phase 2</b> should include new street connections between Colorado Boulevard and Miner Street at approximately the following locations: aligned with 27th Ave., aligned with proposed street connection on north side of Colorado on Sinclair property easement, and aligned with Gilson Ave. This phase also includes the proposed straightening of Miner Street on the eastern end, by Carl’s Jr.	City; Property Owner/ Developer	General Fund (CIP, property acquisition); TIF; Private Development	Ongoing
P1.6	<b>Special Events and Marketing</b> - Work with other city agencies and non-profits to encourage special events in the East End to attract visitors and residents.	City; Non-profits; Business Owners Association	Staff time	Ongoing

## PRIORITY AREA 2: RECREATION AND TOURISM

### WHY IT IS IMPORTANT

As a Colorado mountain community with close proximity to the Denver metro region, Idaho Springs currently lacks the desired recreation amenities that many people seek. While the historic downtown serves as a major tourist attraction, the East End suffers from lack of identity and draw. Creating more opportunities for recreation in close proximity to the city is a primary goal of this plan.

Recreation, in general, refers to outdoor and/or indoor activities such as hiking and biking trails, whitewater sports, climbing gym, sports facility, and other uses that encourage physical activity. Access to recreation is important because it elevates the quality of life for residents and offers daytime activities for tourists which encourage them to stay in the city longer (and spend more money), rather than just a stop-and-go like today. The planned regional Greenway trail will become a major asset to the community and begin to bring people into the city via alternative modes. This trail system can become a “zipper” for the community, where other recreation activities feed into it. It will connect the East End to downtown and to regional destinations east and west of Idaho Springs. A major goal of this plan is to increase the success of the Greenway and enhance its use for non-motorized transportation by providing local connections to it and activating it with proper land use.

Elevating tourism opportunities is also a major goal of this plan. The Argo Mill is seen as a major catalytic opportunity for the East End and Idaho Springs as a whole. This attraction has the ability to gain national attention. This plan supports the redevelopment of the Argo Mill into a mixed use destination with historic resources and heritage tourism opportunities. Another major tourism resource, the Visitor’s Center, is located in the East End. The Visitor’s Center should remain a major stopping point for tourists. Connections to the Visitor’s Center and clear signage directing visitors to it, is important.

### OBJECTIVES

- 1) Create a network of hiking and biking trails in the Virginia Canyon Open Space area.
- 2) Provide connections to the Clear Creek Greenway regional trail system throughout the East End.
- 3) Create recreation amenities that benefits all age groups.
- 4) Seek out recreation use for area adjacent to ball fields.
- 5) Support the redevelopment of the Argo Mill site into a mixed use, historic destination.

### IMPLEMENTATION APPROACH

The approach for implementing the recreation and tourism objectives is to build on the already-planned efforts by offering city resources, such as time, outreach, and funding. Seeking out grants and other funding to support these catalytic improvements is encouraged as well.





## PRIORITY AREA 2 ACTIONS:

	Action	Responsibility	Funding Source	Timeframe
P2.1	<b>Virginia Canyon Open Space Trail Network</b> - Continue working with Volunteers for Outdoor Colorado for preliminary feasibility and design of north hillside trails system on city property. Hire consultant to create trails master plan for area as soon as feasible. Phase trail system in order to implement some trails within the next couple years, with full build-out by 2022.	City	General Fund; GOCO Grant	2017/18 (Design) 2018/19 (Phase 1) 2022 (Full Build-Out)
P2.2	<b>Clear Creek Greenway</b> - Continue planning/design/build-out for Clear Creek Greenway. <b>Phase 1</b> = 10-foot sidewalks along Colorado Blvd. (north side), as part of street project, from intersection of Colorado Blvd. and Riverside Drive to east end of CRC Park (on east end), and from Sinclair to roundabout (on west end). <b>Phase 2</b> = off-street path along Riverside Drive and Clear Creek between CRC Park and two new pedestrian bridges across creek (aligned with Sinclair property easement on east, and CRC Park on west). It also includes smaller connections between Phase 1 improvements at Colorado Blvd. and Riverside Drive intersection and City Hall property (to Waterwheel Park). This phase is anticipated to be funded by a GOCO grant. <b>Phase 3</b> = includes connections from Exit 241 roundabout heading east out of town, crossing creek with new bridge near skate park. This will eventually connect into Denver.	Clear Creek Greenway Foundation; City	General Fund; GOCO Grant; RAMP Fund	2017 (Phase 1) 2018 (Phase 2) 2019 (Phase 3)
P2.3	<b>Other Trail Connections</b> - connect the trail system on south side of I-70 to the greenway trail by way of existing tunnel under I-70 and new connections within the East End. First, provide enhanced wayfinding through signage and directional pavement markings along 24th Avenue, Miner Street, 25th Avenue, and Edwards Street to get people from the trail south of I-70 to CRC Park and Greenway trail. Next, provide an off-street trail around Zone C and D redevelopment areas (when redevelopment occurs) to connect into the Sinclair easement bridge and Greenway trail system.	City; Clear Creek Greenway Foundation	General Fund; GOCO Grants; other grants	2018/19 (Phase 1) Dependent on redevelopment (Phase 2)
P2.4	<b>Clear Creek Greenway Loop</b> - secure easements on private property and create a soft-surface trail route along the south side of Clear Creek that connects between the proposed bridge on east end (aligned with Sinclair easement) to proposed pedestrian plaza/bridge (aligned with 25th Ave.) that connects into the Argo Mill site.	City; Clear Creek Greenway Foundation	General Fund; GOCO Grants; other grants	2020-2022
P2.5	<b>Utilities and Infrastructure South of I-70</b> - assist future developer of land near ballfields with utility and infrastructure needs. Currently, there are limited utilities in the area, which is a deterrent to development. The desired use is for a recreation facility.	City	General Fund; Grants	2019-20



## PRIORITY AREA 3: PARKING MANAGEMENT

### WHY IT IS IMPORTANT

Parking is, and will continue to be a concern in Idaho Springs. The proposed parking structure in downtown will benefit the entire city. While the East End does not currently have parking issues, as new development comes online, it may become a concern. Short-term and longer-term actions to address parking should be planned as a way to support redevelopment and visitor experience.

A goal for the East End is to become a walkable neighborhood. The proximity to downtown allows this district, especially to and from the Argo project, to become walkable. The experience, of course, needs to be pleasant - and plans for improving Colorado Boulevard, Soda Creek Road, and Miner Street will enhance pedestrian comfort, convenience and safety. The city should strive to create a “park once” district by creating parking reservoirs or areas where people can park their cars for long periods of time (as opposed to 2 and 3-hour on-street parking) and easily walk to a variety of destinations. These parking areas need to be easy to find, so signage is important as well.

### OBJECTIVES

- 1) Provide short-term solution for parking that serves Downtown and the East End.
- 2) Create public-private partnership with Argo developers to build a parking structure south of Clear Creek that could serve East End and the Argo project.
- 3) Continue to build out an efficient street network that includes on-street parking.

### IMPLEMENTATION APPROACH

In addition to building a parking structure downtown, the city should be diligent about building a parking network that supports redevelopment in the East End and enhances the visitor and resident experience. Using tax increment funding as a way to assist with parking structure costs for the Argo should be considered. A temporary surface parking lot on the former lumber yard site should also be considered.



## EAST END ACTION PLAN



## PRIORITY AREA 3 ACTIONS:

	Action	Responsibility	Funding Source	Timeframe
P3.1	<b>Temporary Surface Parking Lot</b> - the city should work with the county to clear the site of the former lumber yard. A temporary surface parking lot that serves downtown, the East End, and the hot springs is desired for this location until it redevelops. Consider implementing the pocket park at the intersection of Miner Street and Soda Creek Road as part of this effort.	City; County	General Fund; RAMP Fund	2017
P3.2	<b>Colorado Boulevard Reconstruction Parking</b> - implement Colorado Boulevard phase 3 construction with parallel and diagonal on-street parking spaces. As part of this project, there will be a small public parking lot (34 spaces) at CRC Park. There is also opportunity to expand the parking lot of the Visitor's Center when the tennis court is removed (gain approximately 10 spaces)	City	General Fund; RAMP Fund	2017/18
P3.3	<b>Public-Private Parking Structure</b> - work with the Argo Mill developers on assisting with a parking structure that could serve the East End and the Argo project. The desired location for this facility is just south of the Argo Mill building (south of Clear Creek) on either the Post Office or hardware store lots. The city would need to purchase one of the properties in order to locate the parking structure on this site. The parking structure should be developed as a mixed use project that activates the East End Cultural District along Colorado Boulevard and proposed festival street. The parking structure should be designed to be integrated into the sloped site and shall not block views of the Argo.	City; Property Owner/ Developer	General Fund; TIF; RAMP Fund	2019/20
P3.4	<b>Shared Parking and Access</b> - the city should work with property owners along the corridor to continually push for consolidation of driveway access points onto Colorado Boulevard by means of offering shared access (two adjacent properties share the same access/entrance). Two adjacent properties could also work together to provide shared parking, especially if their peak business hours are different.	City; Property Owners / Developer	N/A	Ongoing
P3.5	<b>New Streets and On-Street Parking</b> - when new streets are built as part of the East End Action Plan, be sure to include opportunities for on-street parking to offer more opportunities for public parking in the East End.	City; Property Owner/ Developer	General Fund	Ongoing
P3.5	<b>Lower Parking Requirements</b> - the East End Overlay Zone includes reduced parking requirements and abilities to implement shared parking and off-site parking, within certain restrictions.	City	N/A	Ongoing

## PRIORITY AREA 4: REVITALIZATION AND ECONOMIC DEVELOPMENT

### WHY IT IS IMPORTANT

A significant portion of the City's capacity for future growth and economic development lies within its eastern gateway in the East End. Unconstrained vacant land is limited; however there are numerous underutilized buildings and sites that offer infill, redevelopment, and adaptive reuse opportunities. The Plan contains a number of test fit sites that illustrate the type and character of development determined to be supportable in the study area. Some existing owners may be interested in reinvesting in their existing buildings to expand and/or make façade improvements. New development firms and investors may also be interested in acquiring existing properties that are for sale to develop new housing and commercial space.

The City desires to support redevelopment and reinvestment consistent with the Plan goals and objectives. It currently lacks the tools needed to encourage and support redevelopment. The creation of an urban renewal authority (URA) is recommended as the most effective entity to allow the City to help accomplish these objectives. A URA is a quasi-municipal authority formed by cities to address “blighted” economic conditions through the use of redevelopment powers including land assembly and tax increment financing (TIF). In order to establish a URA, the City needs to first conduct a blight study to determine which areas in the East End meet the State's statutory conditions of blight. When this step is completed, the City can proceed to completing an urban renewal plan and forming an urban renewal authority.

### OBJECTIVES

- 1) **Promote reinvestment in the East End Plan area.**
- 2) **Reduce the number of vacant or underutilized properties.**
- 3) **Leverage tax increment funds available to spur reinvestment and redevelopment.**
- 4) **Increase tax revenue to better support broader community initiatives.**

### IMPLEMENTATION APPROACH

This revitalization and economic development strategy seeks to increase investment and redevelopment in the East End. This will be done through the creation of an Urban Renewal Authority to facilitate redevelopment and reinvestment in targeted areas of the City, including the East End.



EAST END ACTION PLAN



## PRIORITY AREA 4 ACTIONS:

	Action	Responsibility	Funding Source	Timeframe
P4.1	<b>Blight Study</b> - Hire consultant to complete a blight study to determine the area of the East End eligible to be included in an Urban Renewal Authority.	City	General Fund	Q1 2017
P4.2	<b>Urban Renewal Plan</b> - Hire consultant to complete an urban renewal plan for the proposed area that provides direction on the objectives and planned projects for urban renewal activities	City; City Council	General Fund	Q2 2017
P4.3	<b>Urban Renewal Outreach</b> - Meet with Clear Creek County, Clear Creek County School District, and other affected taxing districts to review the city's intended limited use of TIF. Explain benefits as economic development tool that will benefit everyone, as well as other options looked at as part of the East End Action Plan.	City; County	Staff time	Q3 2017
P4.4	<b>Urban Renewal Authority</b> - Formally establish a URA to facilitate redevelopment and reinvestment in targeted areas of the City including the East End. Hold TABOR election to enable URA to utilize tax increment financing for designated projects consistent with the URA plan objectives.	City; City Council; City Attorney	Staff time; TIF	Q4 2017
P4.5	<b>Form URA Board</b> - Under HB15-1348, a URA Board has 13 members, 10 appointed by the Mayor and City Council, one representative each from the County and Clear Creek School District, and one member representing any special districts.	City	Staff time	Q1 2018
P4.6	<b>Website and Marketing</b> - Hire a marketing and outreach consultant to develop a URA website and related marketing materials to promote development opportunities in the East End.	City; Marketing consultant	General Fund	Q1 2018
P4.7	<b>Developer Forum and Outreach</b> - Invite Denver metro and Front Range developers into Idaho Springs for a seminar/open house to learn about potential development opportunities and incentives. Post development opportunity sites on national websites such as OppSites.com as a way to invite national developers as well. Market the plan area and development opportunities in press releases and publishings about the project and plan area.	City; Outreach consultant	General Fund	2017/2018
P4.8	<b>Matching Grant and Low Interest Loan Program</b> - The city has received a \$25,000 grant from the Henderson Mine for facade and signage improvements. They city should offer matching funds for this program and offer matching grants (up to \$10,000 per project). The city should also work with the Clear Creek Economic Development Corporation (EDC) to offer low interest loans to businesses who seek to improve their properties in a way that aligns with this plan, such as: facade enhancements, new signage, site improvements such as outdoor cafe seating areas and consolidated curb cuts.	City; Clear Creek EDC; DOLA	Grants; Loans	2017/18
P4.9	<b>Argo Mill Site</b> - work with developers of Argo Mill to ensure its success. Provide assistance with seeking out historic preservation and other grant funds to help fill gaps. Also consider utilizing TIF funding through future Urban Renewal Authority to help pay for public improvements such as the pedestrian bridge/plaza and parking structure to support the redevelopment.	City; URA; Property Owner/ Developer	General Fund; TIF; Grants; Tax Incentives	Ongoing

## PRIORITY AREA 5: WHY IT IS IMPORTANT HOUSING

The cost of housing creates challenges for the City's workforce, many of whom may prefer to live in the community but cannot afford to. Housing for seniors will also become a greater issue as the population continues to age and requires more specialized living arrangements. The age and condition of the City's housing stock is also an important consideration, as a large percentage of the City's existing homes are over 75 years old and many are not consistent with current building codes.

### OBJECTIVES

- 1) Expand workforce housing (ownership) options so people that work in Idaho Springs can afford to also live long-term in the city.
- 2) Expand multifamily housing stock (rentals) to offer new housing choices and support the workforce, senior population and ability to age-in-place.
- 3) Expand affordable housing stock to support the city as a place with a mix of incomes and provide a place for all workers.

### IMPLEMENTATION APPROACH

This housing strategy seeks to expand housing choices so that people who work in Idaho Springs can afford to live here and so that people can age in place. This will be done by expanding housing options, including for-sale, entry-level homes geared towards the year-round workforce and their families, rental housing for the moderate-income retail and service workforce, as well as options for seniors who desire smaller, more accessible units and cannot maintain their current homes. There are opportunities to locate many of these housing options in the East End where higher-density residential development can be accommodated.

Some of this housing can be provided by the private market, however the City can also utilize state resources to encourage and support housing development, particularly workforce and low to moderate income housing. The Department of Housing (DOH) at the Colorado Department of Local Affairs (DOLA) provides financial support to developers, public housing authorities, and community organizations to fund various types of affordable housing. This funding is available for new construction, property acquisition, and housing development. Funds are also available through bridge loans and a revolving loan fund. In addition to financial resources, DOH publishes the *Affordable Housing Guide for Local Officials* which contains information on housing programs, funding, and land use strategies and policies to increase affordable housing.

DOH has four major funding programs for affordable housing development – the Home Investment Partnership (HOME) Program, Housing Development Grant Funds (HDG), the Housing Development Loan Fund (HDLF), and the Colorado Housing Investment Fund (CHIF). Local governments are eligible grantees for HOME and HDG funds, which can be used for new construction, property acquisition, rehabilitation of existing units, housing services, and homebuyer or tenant-based rental assistance.



EAST END ACTION PLAN



## PRIORITY AREA 5 ACTIONS:

	Action	Responsibility	Funding Source	Timeframe
P5.1	<b>Incentives</b> - Include in the URA Plan a program to assist affordable housing developers with land assembly and incentives. Develop an incentive program for developers providing income-restricted units meeting the City's housing needs, including flexibility in development regulations and/or reduction/waiving of development fees.	URA; City	N/A	Ongoing
P5.2	<b>Low Income Housing Tax Credit Project</b> - Encourage development of affordable housing project by using tax credits.	City; County; Developer	Low Income Housing Tax Credit program; Developer	2017
P5.3	<b>URA Assisted Housing Development</b> - Develop a strategy to attract the development of a multifamily rental housing project within the URA through use of TIF proceeds and other incentives.	URA	TIF	2018
P5.4	<b>DOLA Assistance</b> - Utilize DOLA Home Investment Partnership (HOME) Program and Housing Development Grant (HDG) funds and other resources to fund and support affordable housing projects.	City	Staff time	2018-19

## PRIORITY AREA 6: BUSINESS DEVELOPMENT

### WHY IT IS IMPORTANT

A key goal is to support businesses that contribute to the community and create a stronger tax base for the city and county. This includes nurturing existing businesses, as well as attracting new businesses to expand the base which in turn will improve the market position and cumulative attraction of the area.

The market study identified the Gateway Commercial Center District as a highway oriented commercial area including potential for complementary uses such as fast casual restaurants, a brew pub, and a limited service hotel. Moving west, the Mixed-Use Corridor and the East End Cultural Center Districts were determined to have opportunities for a small increment of additional commercial space, that could ideally be located as first level uses in mixed-use residential projects.

The attraction of additional businesses will be largely dependent on the provision of new or updated commercial space for lease. The development of incubator or co-working space would be especially helpful to new entrepreneurial businesses. The ability of existing businesses to invest in building expansion and/or improvements will be more dependent on their access to capital which is difficult for small independent entrepreneurs. The city should partner with Clear Creek Economic Development Corporation (CCEDC) and seek funding opportunities at the state through the Department of Local Affairs (DOLA) and the Office of Economic Development and International Trade (OEDIT).

There is a short term window of opportunity for funding through the U.S. Economic Development Administration (EDA) which has funded an economic adjustment strategy - *Clear Creek/Grand County Economic Resurgence and Resiliency Action and Implementation Plan*, associated with the pending closure of the Henderson Mine on Jones Pass to the north of the City on Highway 40. This plan will offer recommended strategies and actions to help each county adjust to the impacts of the mine (in Clear Creek) and mill (in Grand) closure. These actions are directed towards economic expansion and diversification, most of which is likely to occur within the county's municipalities - of which Idaho Springs is the largest. It is therefore important for Idaho Springs to monitor and participate to the degree possible in this study process.

### OBJECTIVES

- 1) Expand highway oriented retail and lodging uses at the East End gateway.
- 2) Provide business expertise and support for existing businesses desiring to expand or diversify.
- 3) Leverage available federal and state funds for economic development and building improvements.

### IMPLEMENTATION APPROACH

This business development strategy seeks to expand retail, lodging, and other businesses in the East End. This will be done through development assistance for existing businesses, as well as a variety of financial support strategies for new development.



EAST END ACTION PLAN



## PRIORITY AREA 6 ACTIONS:

	Action	Responsibility	Funding Source	Timeframe
P6.1	<b>Economic Development Assistance</b> - Access business expansion and attraction economic development assistance	CCEDC; City	N/A	2017
P6.2	<b>Create List of Potential Properties</b> - create a list of vacant and for-sale commercial and industrial buildings and parcels to let prospective businesses know what is available in the city.	City; CCEDC	Staff time	2018
P6.3	<b>Encourage Small Business and Start-Ups</b> - work with the private sector and economic development entities to establish a nurturing environment for small and start-up businesses through special financing programs, incubator office space, and training and support.	City; CCEDC	Staff time; EDC or DOLA grants	2018
P6.4	<b>Economic Resurgence and Resiliency Plan</b> - Monitor and participate in the Clear Creek/Grand County Economic Resurgence and Resiliency Action and Implementation Plan	City; CCEDC	Staff time	2018
P6.5	<b>Colorado Main Street Program</b> - Consider participation in the Colorado Main Street program (this would mainly be for downtown, but would help businesses in the East End as well).	City; DOLA	Staff time	2018

